

BRITISH RAILWAYS

1

GENERAL INSTRUCTIONS
ON
HANDLING, LOADING
SHEETING AND UNSHEETING
OF TRAFFIC DEALT WITH AT
GOODS STATIONS

with notes on acceptance of traffic,
consignment notes, addressing of goods,
insurance, etc.

British Railways Board,
222, Marylebone Road,
London, N.W.1.

September, 1965

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WAGONS AND CONTAINERS.

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ACCEPTANCE AND EXAMINATION OF TRAFFIC

NOTE.—This section consists mainly of brief summaries of instructions for ready reference.

Details are contained in other publications; when in doubt, consult your supervisor.

CONSIGNMENT NOTES.

(a) General.

1. Every consignment accepted for conveyance must be accompanied by the correct form of consignment note completed by senders in accordance with the particulars required as shown on the consignment note.
2. The Board require to have the senders clear assent to the Conditions of Carriage and this must be covered by obtaining senders signature or rubber stamp impression on the consignment note, unless authority has been given by Headquarters to the contrary. Except when so authorised consignment notes must not be prepared by railway staff.
3. Consignment notes referring to third parties for payment of carriage charges must not be accepted.

(b) Description of Consignment Notes.

4. The undermentioned are the forms of consignment note in general use. Other forms are also issued for Agreed Flat Rate Arrangements, Livestock and special kinds of goods :—

For traffic to destinations not entailing customs clearance.

- (i)*Consignment note for general traffic as Board's Risk (white paper, black print—B.R.18790).
- (ii)*Consignment note for general traffic at Owner's Risk (white paper, blue print—B.R.18791).
- (iii) Damageable goods not properly protected by packing (white paper, green print—B.R.18792).
- (iv)*Special consignment notes for explosives, inflammable liquids, poisonous and corrosive chemicals and other dangerous traffic (white paper, red print). (See List of Dangerous Goods and Conditions of Acceptance—B.R.22426 for copies).

For traffic to destinations entailing Customs clearance.

- (v)*Declaration and consignment note—for general traffic at Board's Risk or Owner's Risk (white paper, black print—B.R.18708/1).

The declaration portion must be signed by the sender or his representative.

- (vi)*Declaration and consignment note—for damageable goods not properly protected by packing (white paper, green print—B.R.18710/2).

The declaration portion must be signed by the sender or his representative.

- (vii)*Consignment note—for explosives, inflammable liquids and dangerous goods (the special notes described in (iv) above) and, **in addition**, Customs Declaration for dangerous goods (white paper, red print).

The declaration portion must be signed by the sender or his representative.

Returned empties which have contained dangerous goods.

- (viii)*For empties which have contained certain inflammable liquids or dangerous, corrosive or poisonous chemicals, the special forms of consignment note used for the full containers and referred to in (iv) above, must be used.

***NOTE** :—Sender's own form of consignment note may only be used providing authority has been given by Headquarters.

SPECIAL CONDITIONS RE TRANSIT.

5. Goods must not be accepted from the public for conveyance if any of the printed conditions on the consignment note have been altered or deleted, or if other conditions, such as those given below, have been added :—

“For Tuesday's market”.

“To be shipped (or delivered) tomorrow morning”.

“Load in a covered wagon and send by first goods train”.

Goods must not be accepted if the labels bear similar special conditions.

Special Services are offered, however, which undertake next-day delivery, or conveyance by a named train. Consignment Notes for such traffic should be endorsed by sender to indicate the particular service required.

Except as provided in the preceding paragraph, such remarks should be deleted from the consignment note or label by the sender or his representative, who should sign against the deletion.

This instruction applies also to general orders and to special requests, either verbal or written, made by the public.

6. A promise must never be given that traffic will arrive or be delivered by any stated time, but every reasonable effort must be made to meet the wishes of the public.

TRAFFIC REQUIRING SPECIAL ARRANGEMENTS.

(See also Booklets Nos. 2—B.R.20425 “Instruction for Handling and Loading Specified Traffics”, and 3—B.R.20426 “Instruction and Diagrams for Loading and Securing of Long Projecting and Otherwise Exceptional Loads, etc.”).

7. Articles requiring the use of special appliances, also articles of exceptional weight or bulk, are not to be accepted for conveyance until it is known that both sending and receiving stations can deal with the traffic.
8. The following must not be accepted for conveyance without authority :—
 - (a) Articles and/or loads—
 - (i) exceeding 60 ft. in length ;
 - (ii) which when loaded are equivalent to a load exceeding 60 ft. in length ;
 - (iii) requiring specially constructed wagons or a special train ;
 - (iv) which are out of gauge.
 - (b) Wild beasts or any large animal.
 - (c) Bullion, Specie (Gold and Silver Coin, also Cupro-Nickel Coin of this Realm) and Bank Notes. See B.R. 20458—Instructions to Staff dealing with Bullion Specie and Bank Insaved Packages by Passenger train, Bullion Specie and Bank Notes by goods train.
 - (d) Gold and Silver Plate and statuary.
 - (e) Precious Stones and Platinum.
 - (f) Articles requiring other special arrangements.
9. Consignments consisting of a number of small articles, e.g. spokes, returned empty tins, plough shares, should be packed in suitable receptacles or tied in bundles. Senders should be requested to do this and any difficulties should be reported to the goods agent or his representative
10. Consignments consisting of very small articles, e.g. bricks, tiles and small agricultural pipes, for the Continent and for stations in the Republic of Ireland or Northern Ireland must not be accepted for conveyance unless loaded in containers or in suitable packing cases.

ADDRESSING OF GOODS.

11. Each article or package must (except as provided herein) bear the consignee's full name and address OR a distinguishing mark with a letter-card (or label on letter-card principle) stating on the outside the destination station and on the inside the full name and address of consignee.

WAIT ORDER TRAFFIC.—Each package must bear the name of the person to whose order the package is to be sent and also the destination station.

EXPORT TRAFFIC.—Each package must bear a distinguishing mark, the name of the port or dock of shipment, the name of the shipping agent and the name of the ship, except in those cases where instructions 13(d) and 13(f) apply.

METAL BARS, RODS, TUBES, PLATES, SHEETS, FORGINGS, CASTINGS AND SIMILAR MERCHANDISE must have the full name and address of the consignee on wooden, metal or other durable tallies fastened to the merchandise by wire or, alternatively, painted on the merchandise.

HIDES, SKINS, PELTS.—The labels to be wooden, metal or other suitable material.

Note.—It would greatly assist if all traffic also bore the name and address of the senders who should be requested to show this information on the address label in their own interests as well as those of British Railways.

12. Two or more Articles or Packages.

When a consignment consisting of articles or packages of the same or similar description of merchandise is forwarded to the same consignee, the following provisions apply :—

<i>No. of packages.</i>	<i>No. required to be labelled with full name and address of the consignee.</i>
2 to 10 .. All	} The remaining packages each to be marked with consignee's name and destination station. The total number of packages in the consignment must be shown on each package.
11 to 50 .. 10	
51 to 55 .. 11	
56 to 60 .. 12	
61 to 65 .. 13	
66 to 70 .. 14	
71 to 75 .. 15	
76 to 80 .. 16	
81 to 85 .. 17	
86 to 90 .. 18	
91 to 95 .. 19	
96 to 200 .. 20	
201 to 210 .. 21	
211 to 220 .. 22	

For consignments over 220 packages, one additional fully addressed label to be added for each additional 10 packages or part of 10.

13. Instructions 11 and 12 do not apply to :—

- (a) Empties branded with consignee's name and address. (N.B.—Branded empties consigned to some other consignee must all be fully addressed).
- (b) Full wagon loads to one consignee.
- (c) Consignments of 2 tons and upwards of articles or packages, identical in all respects, from one sender to one station or destination.
- (d) Export and import traffic in through wagons to ship and vice versa.
- (e) Traffic forwarded to Ireland—each package must be fully addressed.
- (f) Traffic for the Continent—the address label on each package must show the name and address of consignee and the names of sending and destination stations.
- (g) Dangerous traffics which are specially provided for.

14. All labels must be legibly written and securely attached. Old labels or addresses must be obliterated before packages are accepted for conveyance.
15. Where for purposes of trade secrecy the railway is requested to remove the address labels at destination station, this must be done before delivery and care taken that the point of origin is not improperly disclosed.

EXAMINATION ON ACCEPTANCE FROM THE PUBLIC.

The following instructions must be observed when receiving merchandise from the public :—

16. Packages should be in good order and as described on the consignment note.
Any damage or discrepancy must be noted on the consignment note; discrepancies must be advised to sender so that he may make the necessary correction before the goods are despatched.
Alterations in the description or quantity of goods shown on the consignment note, or in the address, must be made only by the sender or his representative, who must sign the alterations.
17. Repeated misdeclarations of contents, or under-declarations of weight by sender must be reported to the Divisional Manager.
18. All consignments must be properly addressed (see Instructions 11 to 15).
19. Goods which require to be packed must be packed as laid down in the book of Packing Regulations and Conditions of Acceptance B.R.22435.
Packages, including containers, for the Republic of Ireland, must not contain hay, straw or peat moss litter. Similar restrictions apply to traffic for Northern Ireland, subject to certain exemptions which are notified from time to time.
20. If wrappers be torn or wet, or packages broken, crushed, leaking, or otherwise defective, the contents must be examined to ascertain if there is any damage or deficiency.
21. Casks, drums or other receptacles which are unsound or leaking, or other articles which, owing to their condition, are unfit for conveyance must not be accepted.
22. Empties which have contained dangerous liquids must have been completely drained and all outlets efficiently stopped before acceptance. (See also Booklet No. 2—B.R.20425 “Instructions for Handling and Loading Specified Traffics”).
23. The consignment note must be initialled by the member of the staff who receives and checks the goods at the station.

WEIGHING OF TRAFFIC.

24. Weights declared by senders must be checked whenever possible and any discrepancy noted. Particular attention must be paid to the accuracy of weights declared for merchandise liable to pilferage, e.g. footwear, silk, wines and spirits, tea, tobacco, textiles, hosiery, etc. Different kinds of merchandise must not be weighed together, but the separate weights of each must be ascertained and entered accordingly.
25. When weighing goods in vehicles, care must be taken to see that nothing is weighed with the vehicles which is not to be forwarded by rail.
26. Computed weights must not be accepted unless specially authorised, and when authorised must be frequently tested. Any differences must be reported.
27. Any heavy traffic which cannot be weighed at the forwarding point must be sent to **the nearest point en route** having a wagon weighbridge of sufficient capacity, with instructions to "weigh and advise". If the wagon is overloaded or otherwise unfit to travel, it will be detained at that point **and sending and destination Points advised immediately.**

Every effort must be made to avoid overloading.

RECEIPTS FOR TRAFFIC ACCEPTED.

28. Receipts for goods may be given only at the time of acceptance by the railway and not after the goods have been forwarded. If the goods are in bad or defective condition, the receipt form or sender's delivery book must be endorsed to show the nature and extent of the damage or defect.
29. Grain, hay in trusses, potatoes in bulk and other goods neither measured nor counted, must be signed for as "measure (or quantity) unknown".
30. When a receipt is given for goods sent forward addressed "till called for" a remark must be made on the consignment note that a receipt has been given.

NON-DISCLOSURE OF INFORMATION RE TRADERS' TRAFFIC.

31. Information about the traffic of a trader must not be given to any person not employed by British Railways.

WAGONS AND CONTAINERS

SELECTION.

32. Wagons suitable for the traffic to be loaded should be selected.
33. Covered vans should be used whenever possible for traffic such as flour, sugar, meal, seeds, tea, flax, tobacco and other general merchandise particularly susceptible to damage by wet.
34. Covered vans should not be used for traffic which can be conveyed in open wagons.

35. Containers or covered vans must not be used for traffic of a harmful or objectionable nature if it is likely to make them unfit for subsequent loading.
36. Wagons and containers which have had restrictions placed on their use, e.g.
 - (a) for specified traffics only—meat, fish, motor cars, etc.;
 - (b) for circuit working between two points; or
 - (c) specially labelled for return to a particular point when empty,and only to be used for the specified traffic or between the points indicated. Such stock must never be loaded with general traffic or taken out of circuit without special authority.
37. Privately owned wagons may only be used for the traffic for which they are provided, except when otherwise authorised.

EXAMINATION BEFORE LOADING.

38. Before being loaded or placed at sender's disposal, all wagons and containers must be inspected to ensure that they are in a serviceable condition, e.g. :—
 - (a) The interiors (roofs, doors, sides and floors) must be clean, dry and free from contamination. If a vehicle with a dirty or wet floor has to be loaded, it should be thoroughly swept out and a sufficient quantity of straw or other similar material to prevent goods being damaged, is to be used.
 - (b) Protruding nails must be removed.
 - (c) The roofs, doors and sides of covered vans or containers should be watertight. If they are not, they must be dealt with in accordance with Instructions 49 and 50. Only if no other sound vehicles are available, may they be used provided sheets are utilised as protection from the entry of damp or wet.
 - (d) Doors, fittings and pins must be satisfactory and sliding doors work freely on their runners.
 - (e) The small end shutters provided in some covered vans must be kept closed, except when required to be opened for the loading of long articles through them or for ventilation.
 - (f) Label clips must be examined and defects reported.
 - (g) Old wagon labels and cautionary labels must be removed and old chalk marks rubbed out. Wagon Examiners' red and green cards must on no account be removed).
39. Special care must be taken in examining stock required for such traffics as the following :—
 - (a) Steel billets, forgings, chains, etc.—to see that the floors are not defective.

- (b) Traffic liable to damage by wet—to see that the stock is watertight.
- (c) Grain, flour, fruit, sugar, tea, potatoes and other foodstuffs, animal or human; tobacco, cigarettes, etc.—to see that the stock is clean and free from taint, i.e. has not recently been used for mineral oils, naphtha, tar, wet skins, fish, bones, lead ashes, white lead or other lead compounds, or poisonous or offensive traffic of any kind which leaves a taint.

The interiors of meat vans and containers, including hooks and bars, must be clean.

CLEANING AND DISPOSAL AFTER UNLOADING.

40. After unloading, all wagons and containers must be properly swept out and cleaned as may be necessary.
- (a) The interior of meat vans and containers (including hooks and bars) must be cleaned.
 - (b) All hay and straw used as packing for goods received from the Continent in train ferry wagons or containers must be disposed of immediately under local arrangements.
 - (c) Wagons which have conveyed mineral oils, naphtha, tar, wet skins, lead ashes, white lead and other lead compounds, poisonous or offensive traffic of any description, or articles liable to taint foodstuffs, must be thoroughly cleansed at DESTINATION STATION be scraping, sweeping or washing.

If this cleansing cannot be done effectively at the destination station, the wagon must be sent to the nearest cleansing depot for treatment, bearing wagon label B.R.21208 completed with the additional information indicated on the specimen below.

USE BLOCK LETTERS 19	B.R. 21208
From		
<h1 style="margin: 0;">EMPTY</h1> <h2 style="margin: 0;">FOR CLEANSING</h2>		
To		
Letter	Number	
Wagon		
Container		
PREVIOUS TRAFFIC -		

Wagons are sent for cleansing to :—

- (i) Livestock Depots.
- (ii) Carriage and Wagon Works.

The Cleansing of Wagons sent to Livestock Depots must also follow the procedure laid down under heading “Cleansing Methods”.

Wagons which have been used for conveyance of Acids, Bones, Creosote, Creosoted or Wolman Tanalath treated sleepers, Fat, Fertilizers, Fish, Fish Meal, Fish Offal, Gas Lime, Glue, Hides, Skins and Pelts, Mineral Oils, Naphtha, Nitrate of Soda, Paint, Sheep Dip, Tar, Tar Macadam or any traffic which leaves taint in the wagons must not be used for foodstuffs or any other traffics until the chances of contamination are negligible.

Mineral Wagons, especially those with steel bodies, should be used, where possible, for the conveyance of obnoxious and dangerous traffics such as Bones, Cullet, Fish in bulk, Spent Oxide, etc., to facilitate cleansing operations, contamination of ordinary merchandise rolling stock will also be obviated.

For particularly obnoxious traffics, especially where short hauls are involved, circuit working of wagons obviates cleansing operations. Where economies would appear to be possible, full details of regular flows should be submitted for consideration.

The co-operation of other Departments using revenue-earning wagons should be enlisted in cleansing them before handing back empty.

CLEANSING METHODS (other than Cattle Wagons). SPECIAL NOTES :—

1. The following list of traffics is not intended to be exhaustive, but gives an indication of traffics most frequently carried which contaminate vehicles.
2. It is not intended to cover ordinary traffics where the vehicles can be cleansed by normal sweeping out operations.
3. If any doubt arises as to correct treatment of vehicles after conveyance of dirty traffics, whether or not listed below; the Chemist's advice should be sought.

<i>Traffic</i>	<i>Clean- sing Points</i> ²	<i>Cleansing Method</i>
Barytes Basic Slag Bauxite Residue Bricks and Tiles Carbon Black Cement Charcoal Copperas, Green Cullet Fertilizers Gas Lime Lead Ashes Lime Limestone Marl Ochres Ore Oxide of Iron, Waste Oxide of Iron, Spent Oxide of Iron from Gas Works Oxide of Iron for Gas purifying Oxide, Burnt, Spent Oxides, Red, Yellow Plaster Pyrites Rock Phosphate Salt Sand Soda, Soda Ash, etc. Sulphur Tanners Extract Vegetables in bulk White Lead Powder Zinc Residues	A B†	If dry, A to brush out. If brushing out at desti- ation station does not satisfactorily cleanse the wagon, or if the residue has been wetted send to B for scraping, hosing and brushing.
Bananas	A	Brush out and Burn straw.
Clay, Ball, China Fire Gannister (Silica Cement)	A	Scrape and brush out.
Hides, Pelts and skins (Dry) Hoofs and Horns (Dry) Sugar Beet Pulp	B†	If necessary to be sent to for scraping, brushing and hosing.

<i>Traffic</i>	<i>Clean- sing Points *</i>	<i>Cleansing Method</i>
Drainage from oily turnings Spillages of oil, tar, creosote, paint, glue, etc.	A	As first aid destination stations to spread sand or sawdust, then scrape and brush.
	C	If destination station unable to treat send to C for treatment.
Bones and Fats Fish, Fish Meal and Offal Hides, Pelts and Skins (Wet) Hoofs and Horns (Wet) Spetches Scutches, etc.	B†	Scrape and hose, Treat woodwork with Sodium Hypochlorite solution such as "Chloros", diluted 1 to 5 with water. Allow to act for ½ hour, then hose. If available steam may be used instead of Chloros.
	A	If wagons are not reserved for this traffic, destination station to brush out. If wagon is not oil stained, to be kept out of foodstuffs traffic for one month.
Creosote Salts (Naphthalene Flakes) Naphthalene Spillages etc. of traffics listed in Dangerous Goods Classification	C	If woodwork is oil stained, destination station to send to C for treatment.
	C	Where advice will be given by chemist.

*A—Destination Station.

B—Livestock Depot.

C—Carriage and Wagon Depot.

†—In the case of Southern and Western Regions this may involve sending to C.

- (d) Railway or privately-owned wagons received loaded with bricks, coal, dross, tarred roadstone, stones etc., must be completely cleared by the receivers before being sent away as empties.

41. Wagon sheets must be removed and dealt with in accordance with Instructions 145, 147 and 163.
42. When wagons or containers have been received sealed, the seals must be completely removed **from both sides**.
43. Nails, loose equipment, etc., used for packing or securing merchandise must be removed. Care must, however, be taken to see that equipment belonging to the wagon or container, e.g. straps, wheelbars and steel spiked scotches on motor car trucks, cross bearers or side baulks on certain special trolleys, spreader bars on open "C" and "D" type containers, etc., is properly replaced.

REMOVAL OF FIXED CHAINS IS PROHIBITED.

44. Chains, etc., with which conflat and other wagons are fitted, when not in use for securing a load, must be safely stored in the pockets, boxes or wells where these are provided, or, where there is no such provision, must be made secure as follows :—
 - (i) On wagons with sound floors and having sides $3\frac{1}{2}$ " or more in height, the chains must be inside the wagon and clear of the sides and ends.
 - (ii) If there is any defect in the wagon floor or the sides are below $3\frac{1}{2}$ " in height, chains must be hooked or secured together across the vehicle in such a way as to prevent them moving or falling off during transit.

Chain boxes and wells must be examined and defects reported to the wagon examiner, special care being taken to see that there is no hole in the floor through which chains might work whilst in transit.

Other appliances and fittings on special wagons must be made quite secure and safe.

45. Doors, including bottom doors, must be properly closed and fastened.
46. Wagons, containers, sheets and ropes must be promptly released and recorded on the appropriate freight rolling stock returns. "Spare" stock must not be retained in expectation of traffic.

EXAMINATION OF EMPTY WAGONS.

47. "Empty" wagons must be examined and any addressed merchandise found in them forwarded to the destination, a report of the circumstances being made to the Divisional Manager. (For unaddressed merchandise, see Instruction 115).
48. Sheets, portable sheet supporter bars and bows and loose packing or securing equipment (i.e. chains, etc., not forming part of the equipment of the wagons) must be removed and dealt with in accordance with instructions.

DEFECTIVE WAGONS AND CONTAINERS.

49. The attention of wagon examiners at depots where such staff are employed, must be called to defects in wagons and containers. If necessary, the examiners will arrange for the stock to be labelled to the nearest wagon shops for repairs.

Where a wagon examiner is not employed, the Goods Depot staff should, when a wagon is considered defective, arrange for the attention of the nearest wagon examiner to be drawn to it. If a container is so defective as to be unsuitable for use, it must be labelled to the nearest wagon shop for repairs, the defects being shown on the label.

50. A special "Inspection for Leakage" label B.R.21215 is provided for labelling, **to the nearest repair shops**, vans and railway-owned containers with defects which might result in goods being damaged by wet if used in traffic. These labels are to be used by the Goods Depot staff in the following circumstances :—
(a) at stations where a wagon examiner is not located; and
(b) at stations where difficulty is experienced in making contact with a wagon examiner.
51. Any station receiving a covered van or container with the roof covered by a sheet, must, after unloading, remove the sheet and, if the van or container shows any sign of defect, it must be dealt with as instructed in Instructions 49 and 50.
52. Wagons, or containers, which are marked or labelled for repairs (Green carded) or are obviously defective must not be loaded, except railway wagons with the repairs label endorsed "May be loaded". These may be used in the direction of the repairing shops.

HANDLING, LOADING AND UNLOADING

(See also Booklet No. 2—B.R.20425 Instructions for Handling and Loading of Specific Traffics, and Booklet No. 3—B.R.20426 Instructions and Diagrams for Loading and Securing of Long and Projecting and Otherwise Exceptional Loads, etc.).

PERSONAL SAFETY.

53. Staff should avoid putting themselves in any position involving unnecessary danger and should take care to avoid injury to themselves or to others. (See "Your Personal Safety" Booklet, (B.R.7013/5).

CARE IN HANDLING.

(a) General Instructions.

54. When traffic is labelled "Brittle", "This side up", "With care", etc., or when consignment notes or invoices show that the goods are liable to be damaged, special care must be taken in handling and loading and the instructions on the labels or documents must be carried out.

A standard code of pictorial markings of packages has been recommended by the British Standards Institution for guidance on the handling of **non-dangerous** goods.

These designs have already been accepted on the Continent and are being increasingly used by British firms; they are shown below to enable handling staff to become acquainted with the six Symbols agreed by British Railways.



FRAGILE—
HANDLE WITH CARE



USE NO HOOKS—
DO NOT PUNCTURE



THIS WAY UP



SLING HERE



HEAVY WEIGHT
THIS END



KEEP COOL—
STOW AWAY
FROM BOILERS

55. Hand hooks, or lifting equipment, must not be used for moving goods liable to be damaged thereby.
56. All large casks, including those containing breakable goods such as glass, tiles, etc., should be rolled.
57. Open-topped packages must be kept right side up and level; packages liable to cause damage must not be placed on top.

58. To prevent damage to other traffic, protruding nails should be withdrawn from scotches and timber which is being returned to sending point. If the nails cannot be withdrawn, they should be bent over and hammered down.
59. Care must be taken that address labels are not torn off or mutilated by packages being dragged instead of lifted off other goods.
60. When moving goods by hand or crane, care should be taken to avoid knocking over or damaging other goods.
61. Traffic deposited on the ground must be at least 5 ft. away from the nearest rail.
62. Care must be taken that documents, particularly consignment notes and delivery sheets, do not get astray or mutilated.

(b) When using Barrows.

63. When loading barrows, the load must be so constructed that damage by crushing or by articles falling off is avoided. If articles do fall off, they must be picked up at once and placed in a safer position.
64. Packages must be unloaded from barrow by hand, not tipped off.
65. Loading boards, or plates, should always be used :—
 - (i) When barrowing into and out of wagons not fitted with drop doors or where the drop doors are not bevelled; and
 - (ii) when barrowing between road vehicle and deck.

(c) When goods are damaged.

66. Goods in a damaged condition should be handled with special care.

Leaking casks, torn bags and other damaged packages must be repaired to prevent further loss of contents or damage to other goods. If necessary, the services of a cooper should be obtained.

67. Particulars of the damage and the probable cause must be recorded. Serious damage must also be brought to the notice of your supervisor or the claims representative so that liability may be determined, and, where possible, preventive measures taken for the future.
68. When the damage has been caused by wet, a report must be sent to the forwarding station. The forwarding station must take whatever action may be necessary to prevent similar damage to other traffic.
69. Where packages are not in the same form after repair, e.g. if one package is split into two or more, or two or more are converted into one, arrangements must be made for the destination station to be advised of the change.

70. When leakage of wines or spirits under bond is discovered, the action detailed in Instruction No. 2—Booklet No. 2 B.R.20425 “Instructions for Handling and Loading Specified Traffics”—must be immediately carried out.

(d) Use of Special Appliances.

71. To avoid damage to traffic, appliances are specially provided at some stations for the handling of such goods as glass in crates, heavy cases, etc., and for conveying traffic from deck to wagon. Where available, these appliances must always be used when dealing with the traffic for which they are provided.

CRANING.

(See also Booklet No. 6—B.R.20429 “Instructions to Staff Using Mechanical Appliances”—and regional directives which contain additional instructions on the use and maintenance of lifting appliances and loose lifting tackle).

72. Proper tackle must be used and must be securely attached to avoid goods falling during craning.

Goods must be lifted and lowered steadily and slewed carefully.

73. The safe working load is stamped on the end link of all chains and on the main end link or ring of chain slings, also on all hooks, swivels, rings and shackles.

Ropes bear a ferrule marked with the safe working load or the circumference. Stations using ropes marked with the circumference are provided with a table showing the safe working loads for them.

Special care must be taken when using a sling, as the safe working load varies with the angle between the legs of the sling. A table is provided showing safe working loads with the legs at different angles.

The safe working loads for lifting appliances and tackle, as shown thereon, or in the safe working load tables, must not be exceeded.

74. Before any rope, sling or other lifting tackle is used it must be examined to see that it is in a satisfactory condition. If frayed, or damaged, it must be taken out of service.

75. To prevent fragile traffic being torn or crushed, use must be made, as necessary, of rope slings, spreader bars or other appliances provided.

Slings and chains must be kept from pressing on the loads, packing being placed between them and the loads if necessary.

76. Securing ropes, chains, etc., on loads of timber or other material for bulk lifting by crane must not be released until the lifting tackle has been attached and the strain taken by the crane.

77. Staff should avoid putting themselves in any position involving unnecessary danger where cranes are being used (See “Your Personal Safety” Booklet B.R.7013/5).

LOADING GAUGE.

78. The height and width of the load, as well as the dimensions of the wagon, must not exceed the gauge of the lines over which the wagon will travel. Particulars of the railways gauges are given in Booklet No. 9—B.R.20873, "Dimensions of Loads" Other prohibitions on the forwarding of certain types of wagons, loads, etc., to stations and private sidings are also issued.
79. Vehicles of special types constructed to carry heavy or bulky articles should not be loaded until it has been ascertained that they can be safely worked to and dealt with at destination.
80. Any consignment which—
- (i) cannot be loaded within gauge in all respects; or
 - (ii) exceeds 60 ft. in length; or
 - (iii) when loaded is equivalent to a load exceeding 60 ft. in length; or
 - (iv) is of exceptional weight, or bulk in proportion to weight— must be dealt with in accordance with the directives contained in Booklet No. 3—B.R.20426 "Instructions and Diagrams for Loading and Securing Long, Projecting and Otherwise Exceptional Loads, etc."

CHECKING.

81. Goods must be checked with the appropriate documents, as shown below :—

Forwarded traffic.—Consignment note or other forwarding document. Particulars of each article of package without a consignment note or forwarding document, must be recorded.

Goods must be carefully examined to see that they are in good condition and as described on the document.

Marks and numbers should be checked.

The Wagon number into which the traffic is loaded to be shown on the consignment note or forwarding document.

Received traffic.—

(A) Sundries (consignments of less than 1 ton).

(i) Compilation of goods delivery sheet.

Particulars of all shed traffic loaded to road vehicles to be entered on a separate Goods Delivery Sheet (B.R. 17111/1) in duplicate for each load by the respective checkers, the information thereon, to be taken from the address labels, to show the following :—

Sender's name and full address.

(Traffic put on rail by British Road Services—"B.R.S." and name of depot, as indicated on the special adhesive labels, also to be shown).

Consignee's name and full address.

Number of articles loaded to road vehicles.

Description of packages, marks, and contents when known.

“PAID HOME” when so labelled in the case of traffic for out-boundary cartage.

Remarks as to condition of goods on carbon copy only.

It is imperative that the sheets be accurately and legibly prepared.

(ii) **Traffic not for immediate delivery.**

Traffic, as under, not for immediate delivery, must be conveyed to allocated posts :—

- | | |
|---|--|
| (a) Traffic received without address. | } Separate Goods Delivery Sheet for each type (a) to (f) to be compiled—Weight of all items must be shown. |
| (b) Addressed “To be called for”. | |
| (c) “Wait Order” traffic. | |
| (d) Traffic which is known to be collected by consignees. | |
| (e) Warehouse traffic. | |
| (f) Traffic delivered by rail wagon. | |

The Goods Delivery Sheet for traffic “To be called for” to be available throughout the day at the berth to obtain signatures for traffic collected currently, but at convenient intervals the sets of duplicate sheets (a) to (f) must be taken to Delivery Office.

When traffic is connected to a delivery order, a release note to be compiled and sent to order checker, who will take goods and release note to the cartage vehicle checker, who will enter consignment on Goods Delivery Sheet and attach release note thereto.

(iii) **Checker’s Marks.**

To obviate entering separately a number of packages for one consignee from the same sender, checkers should use their usual method of check on Goods Delivery Sheet in special column provided for the purpose, viz. 111, i.e. 5 or “Gate” check and enter the total number when load is completed.

(iv) **Vulnerable Traffic.**

Where conveyed under Full Security arrangements, the list compiled at the forwarding point is to be checked with the traffic at wagon side. Goods Delivery Sheet to be compiled at the cartage vehicle front.

For traffic under Full Security and not listed at the forwarding station, a record of the traffic received in each full wagon load of vulnerable sundries is to be

taken in the prescribed form at the wagon side and the attention of the forwarding station drawn to the omission.

For traffic not conveyed under Full Security, no special documentation is required.

(v) Outboundary Charges.

Where weighing cannot be performed, the dray checkers to insert estimated weight on the Goods Delivery Sheet to enable outboundary charges to be raised.

(B) Full Loads (consignments of 1 ton or over).

(i) Traffic to be delivered direct to consignee.

Delivery sheet. In the absence of a delivery sheet, an “unentered” slip to be compiled, giving full particulars.

If consignee’s address is not given on the delivery sheet, it should be obtained from the address label and written on the sheet.

(ii) Traffic to be placed into storage.

Invoice. In the absence of an Invoice an “unentered” slip to be compiled, giving full particulars.

(iii) When traffic is regularly received without invoice or other document to which it may be checked, the Divisional Manager must be advised.

(iv) Invoiced weights of received traffic must be checked as frequently as possible.

82. Traffic transferred to another station by road vehicle should be accompanied by a document showing details of the consignment, viz. destination, number and type of packages and description of goods, for identification purposes.
83. Special care should be taken in checking the total number of pieces in consignments, e.g. pipes, bends, connections and similar small articles, the “gate” system of checking being used.
84. Where possible the letter and number of the wagon in which forwarded traffic is loaded must be inserted on the document. Where more than one wagon is used for one consignment, the quantity loaded in each must be shown.
85. Special attention must be given to the condition and weight of textiles, wines and spirits, clothing, tea, tobacco and cigarettes, and other traffic particularly liable to theft. The weight of any packages in loose condition or showing signs of leakage must be tested on unloading and if the merchandise is not at once delivered it must be re-weighed immediately prior to delivery.
86. Any deficiency in packing, bad addressing, discrepancy, defect or damage, should be noted on the document (consignment note, invoice or other document) and initialled by the checker.

87. The notice of the supervisor must be called immediately to any signs of pilferage, theft or re-labelling of goods and also to all discrepancies in merchandise for shipment and perishables.

Suspected pilferage, theft or re-labelling of goods must be reported to the Railway Police and the Divisional Manager in accordance with instructions.

Where the state of the load shows signs that missing goods may have fallen off and theft is not suspected, this must be reported to the Divisional Manager only.

88. All documents relating to traffic dealt with should be initialled by the individual who performs the checking and passed to the appropriate office immediately they can be released.

Care must be taken to see that no Goods Delivery Sheets are left on the shed at close of work.

Note.—Detailed instructions relating to the checking and documentation of consignments of less than 1 ton are contained in Booklet B.R. 17'11 (Revised)—“Abolition of Invoicing”.

LOADING AND UNLOADING WAGONS AND CONTAINERS.

89. The proper loading of traffic in wagons is of great importance both in avoiding damage to the traffic and in preventing goods from becoming displaced or falling on the line, with the consequent danger of serious accident.
90. The plated carrying capacity on the side of each wagon or container must not be exceeded.
91. Wagons must be loaded in such order as will prevent unnecessary shunting and must be ready for the train appointed to take them.
92. Every endeavour must be made to load each wagon to its full capacity. The loading of certain specially constructed vehicles is subject to headquarter's authority.
93. Before loading commences, destination indication boards, where provided, should be fixed in prominent positions.
94. **The propping of wagon doors** for the support of portable weighing machines or for loading or unloading of any description, or for any other purpose, **is prohibited.**
95. Care must be exercised to ensure loading to correct destination or tranship point.
96. Separate parts of a consignment must, as far as possible consistent with full and safe loading, be loaded together in the same wagon. When it is necessary to load a consignment in more than one wagon, the number of packages in each wagon must be shown on the consignment note.
97. As far as practicable, wagons should be loaded from end to end and the load made level and compact, with the weight evenly distributed.

Articles liable to move and damage other goods should be secured in position.

Light or fragile goods should be loaded so that heavy articles do not press on or against them, and care taken to avoid damage to paper sacks.

Goods likely to damage others by contact should be loaded apart.

Goods must be loaded in such a manner that they will not fall out when the wagon doors are opened, or be damaged when the doors are opened or closed; long articles should not block the doorway on either side.

98. Straw, straw pads, rope rings or other suitable packing should be used, where necessary, to prevent damage by denting, chafing or scratching and also to prevent movement of goods. Bales, trusses, glazed sinks, rolls of linoleum or carpets, paper, etc., which may get dented, chafed or scratched, must not be loaded against bolts or other projections inside wagons.
99. Loads in open wagons should be built so that hollow sheeting may be avoided and to prevent any article falling off in transit.

They must be secured, where necessary, to prevent the possibility of displacement. Safety must always be first consideration and if there is any doubt, loads must be roped.

Dry ropes must be used whenever possible.

When loads to be sheeted require roping, the ropes should, if practicable, be placed under the sheets.

100. Before despatching open wagons in which straw or other similar packing is used, all unnecessary packing must be removed from the top of the load.
101. When any wagon partly loaded has to be shunted, the traffic in it must be made secure so that it will not be damaged during the movement.
102. Upon completion or close of work, all wagon doors should be closed and door pins properly secured.

Goods liable to damage by wet.

103. These goods should, where possible, be kept away from the doors of wagons and the small end shutters (where provided). When loading close to a door cannot be avoided, straw must be packed between the goods and the door.

Goods liable to theft.

104. Valuable goods and small packages liable to theft, such as tobacco, cases of wines and spirits, should be loaded in the middle of the wagon and surrounded by other goods. They should be kept away from the small end shutters (where provided).

The arrangements for the locking, sealing, unlocking and unsealing of wagons and containers of vulnerable traffic must be strictly observed.

Valuable goods and machinery.

105. This traffic, and any bulky goods liable to be damaged or to cause damage to other traffic, must always be loaded under the supervision of a foreman or other experienced man.

Separation of foodstuffs, foodstuff empties, etc., from objectionable traffic.

106. In addition to the dangerous, corrosive and poisonous chemicals specified in the "List of Dangerous Goods B.R.22426", other traffic of an offensive nature, such as wet hides, ice, kits of fish, mineral oils, paints, naphthalene, soil insecticide, disinfecting powder, creosote, ink, etc., must not be loaded close to bacon, sugar, flour, meal, or other foodstuffs, foodstuff empties, hops, tobacco, or goods liable to become tainted or damaged by odour, leakage or drainage.

Rail vehicles not under cover.

107. When wagons are loaded or unloaded in the open, goods likely to be damaged by exposure to the weather should be protected.
If sheets have to be folded back for partial loading, unloading or examination of the load, they must be replaced when the work is completed or suspended, so that the contents of the wagon are fully protected from wet.

Received traffic.

108. All wagons must be unloaded promptly.
109. Covered vans sealed or padlocked must be carefully examined on arrival and should anything be noticed amiss with the fastenings, the defect must be pointed out to your supervisor so that the Divisional Manager and the Railway Police may be informed.
Sheets and ropes should be carefully taken off, the sheets being dealt with as shown in Instructions 145, 147 and 163, and the ropes coiled and put in the appropriate store.
110. Care should be taken when opening wagon doors that packages do not fall out or get damaged.
111. After unloading, wagons and containers should be dealt with as shown in Instructions 40 and 42 to 46.
Chains, packing, scotches, etc., which are returnable must be dealt with in accordance with instructions.
112. Overloading, improper loading or defective sheeting or roping must be reported to your supervisor so that the irregularity can be taken up with the station at fault.
113. If traffic received in a covered wagon or container has become wet through a defect in the wagon or container, the place where the wet has got through must be chalk-marked and the wagon or container dealt with as defective. (See Instructions 49 and 50)
114. If traffic is received insufficiently addressed, or without address, the wagon number and date of receipt should be chalked on the package by the unloading staff and the goods directed to the "goods on hand" berth.

115. Goods wrongly received should be sent to correct destination by the most speedy transit. Full load traffic must be accompanied by a "temporary" invoice giving full particulars. A record or copy of the temporary invoice is to be retained so that sending station may be advised of irregular loading and destination station of the whereabouts of the traffic.

LOADING AND UNLOADING ROAD VEHICLES.

116. Before loading or unloading any road vehicle at a deck, the wheels should be secured to prevent movement.
The floors of empty vehicles should be clean and in sound condition; if not dry, straw or suitable material must be used to avoid damage by wet.
117. Where vehicles are loaded or unloaded in the open, goods likely to be damaged by exposure to the weather should be protected.
118. Packages should be loaded on to road vehicles in such a manner that they will not fall off and as far as possible in round order so that they are readily available when required for delivery.
119. The weight distribution and stowing must be such that no person on the vehicle or on the road is endangered.
On articulated trailers the weight must be distributed as evenly as possible and on no account must heavy packages be loaded on the front corners, unless the trailer is adequately supported at the front end.
120. Goods liable to contaminate foodstuffs or damage each other should not be loaded together without suitable protection being given.
Valuable traffic must be so loaded that theft, pilferage or damage is prevented; full loads should be sheeted.
121. Vulnerable traffic should be dealt with in accordance with instructions.
122. Packages found at the cartage vehicle loading berth unaddressed or without indication of ownership should be brought to the notice of the discrepancy tracer or foreman.
123. When traffic which cannot be delivered for any reason is brought back to station, an appropriate record must be made so that the goods may subsequently be identified.
124. When road vehicles are unattended, they must be so placed that there is a clearance of at least 5 ft. from the nearest rail.

CARE AND CUSTODY OF TRAFFIC HELD BACK.

125. Valuable traffic or traffic liable to pilferage held over must be placed in safe custody in the "lock-up" or held in locked vans.
126. Wagons partly loaded or unloaded and held over should be trimmed for safety; open wagons which are likely to be moved into the open yard must be sheeted when necessary.

127. At forwarding stations, partly loaded wagons requiring to be re-set for completion of loading must be labelled, or over-labelled, with the "SHUNT BACK FOR LOADING TO BE COMPLETED" label (B.R.21222).

At destination stations BOTH wagon labels must be retained on loaded wagons until the whole of the contents have been unloaded.

128. Water or snow which has collected on hollow-sheeted wagons standing under load in station yards must be removed and frequent examination of the traffic must be made. If any becomes damaged by wet it must be dealt with specially.

CARE OF STATION EQUIPMENT.

(See also Booklet No. 6—B.R.20429 Instructions to Staff using Mechanical Appliances, Regional directions on Loose Lifting Tackle, and Booklet No. 8—B.R.20431 "Instructions for the Operation and Maintenance of Weighing Machines and Weigh-bridges").

129. Station equipment must only be used for the purpose for which it is supplied and after use must be returned to the appropriate place for storing. It must not be taken or sent from the station without authority.

Care must be taken to see that shed barrows, loading plates, portable lamps, wagon indication boards, etc., are removed from rail vehicles before the vehicles are despatched.

130. Ropes, chains and packing supplied to stations for the purpose of securing merchandise whilst in transit must be kept in a dry place. Ropes must be coiled and screw chains oiled and kept in proper condition.
131. Portable sheet supporter bars, packing, rope scotches, etc., returnable to forwarding or owning station must be promptly returned, properly labelled.

Arrangements must be made for the prompt return of ropes, chains and other tackle sent to private sidings. Any undue detention must be reported.

132. Worn out or damaged ropes, chains and packing, and ropes which have lost their ferrules, must be returned to the stores, or as otherwise instructed, for replacement or renewal.
133. Sheets must be dealt with in accordance with Instructions 145 to 148.
134. When goods terminal equipment is sent to works for repairs, the following information must be shown on the label :—
- (a) Date of despatch.
 - (b) Forwarding point.
 - (c) Date and number of requisition.

LABELLING OF WAGONS

GENERAL INSTRUCTIONS.

135. All loaded wagons, except those containing mineral traffic in train loads for one destination, must be labelled on both sides.

When it is necessary for any empty wagon to be labelled to its destination, it must be labelled similarly.

Where practicable, wagons should be labelled on commencement of loading but the date must not be inserted until day of despatch.

136. Labels with destination printed should be used whenever available; hand written labels must be made out in block letters with thick black pencil.

All the information for which space is provided on the label must be inserted (except as shown in Instruction 140(b)).

137. When label clips are provided, the labels must be placed in them; if label clips are not provided, the labels must be securely tacked to the wagon in a conspicuous position but protected as far as possible from rain.

All labels must be affixed in positions where they will be visible after the loads have been sheeted.

138. Each wagon must bear labels which show, in addition to the name of the destination station, the route code applicable to that station, except "out-of-gauge" labels and labels on loaded wagons with axle weights in excess of $17\frac{3}{4}$ tons when the full route must be shown.

The load category should be indicated, by a letter, in the box provided—'H' (Heavy), 'M' (Medium), or 'L' (Light), respectively, wagons loaded to $\frac{3}{4}$ carrying capacity and over, $\frac{1}{2}$ to $\frac{3}{4}$ carrying capacity, and under $\frac{1}{2}$ carrying capacity. Wagons of sundries, returned empties, and single A/B/BD containers should be designated 'L'.

STANDARD LABELS.

139. (a) (i) **Ordinary**—These are printed in five types, as under :—

Coal	Load Category	H	B.R.21201
Other traffics	" "	H	B.R.21200/3
" "	" "	M	B.R.21200/4
" "	" "	L	B.R.21200/5
" "	" "	Blank	B.R.21200/2

(ii) *Special (General) and Cautionary*.—The following special labels and gummed target slips are provided and should be used where applicable :—

LABELS

<i>Description</i>	<i>B.R. Number</i>
Acids in Glass—Shunt with care.	21204
Bonded Goods under Customs and and Excise Lock or Seal.	21205
Container must not be lifted.	21207

Dangerous Goods.	21276,	21280, or 21336
Empty.		21208
Empty Cattle Wagon—Cleansed and Disinfected.		21209
Examine Load.		21210
Explosives.	21273 to	21275
Fish.		21211
Folded Sheets.		21212
Highly Inflammable.		21279
In circuit working, empty.		21214
Inflammable.	21278 or	21279
Inspection for leakage.		21215
Livestock.		21216
Potatoes.		21217
Radioactive.		21335
Returned Empties.		21218
Ropes.		21219
Shunt back for loading to be completed.		21222
Shunt with care.		21221
Special Vehicle urgently required.		21223
Sugar Beet.		21225
Tinplates and Blackplates—Shunt with great care.		21226
To be Weighed at	21227,	21228, 21229
To Wait Orders.		21330

GUMMED TARGETS

<i>Description</i>	<i>B.R. Number</i>
Exports Express.	21327
Glass with great care.	21231
Household Removal.	21232
Perishable.	21233
Shipment.	21234
Urgent.	21235

(iii) Special (Regional) labels for certain flows of traffic peculiar to individual regions are supplied for use at particular stations.

- (b) “SHUNT WITH CARE” labels (B.R.21221) should be affixed to all wagons containing earthenware, eggs, furniture, glass, light castings, wines and spirits, or other fragile traffic. These labels should be securely fixed in prominent positions near the destination labels and also at each end of the wagon over right-hand buffer. Such labels for vans with plywood bodies, all steel and steel-ended wagons must be affixed to the wooden boards where provided for this purpose.

- (c) "EXAMINE LOAD" labels (B.R.21210) should be used on wagons conveying loads which require special examination enroute. (See also Instruction No. 6, Booklet No. 3—B.R.20426—Instructions and Diagrams for Loading and Securing Long, Projecting and Otherwise Exceptional Loads, etc.).
- (d) "URGENT" targets (B.R.21235) are only to be used for traffic specially coming within that category; they must not be used for general purposes.

SPECIAL REQUIREMENTS OF CERTAIN TRAFFICS.

140. (a) **Full load traffic** (other than traffic particularly liable to theft—see (b) below)—Must have on the labels the name of the consignee and description of the contents of the wagon.
- (b) **Traffic particularly liable to theft** (vulnerable traffic)—The labels of full wagon loads of this traffic, e.g. tobacco, cigarettes, wines and spirits, hosiery, drapery, confectionery, preserves and groceries, must not show the contents of the wagon.
 "Perishable" targets (B.R.21233) are, however, to be used when applicable.
- (c) **Agricultural Show Traffic**—The labels of full wagon loads must be endorsed "Show Traffic".
- (d) **Grain, etc., Traffic**—Code letters or numbers, contract numbers, lot numbers, etc., when shown on the consignment note and the kind of grain, e.g. wheat, barley, etc., must also be shown on the wagon labels.
- (e) **Petroleum Rail Tank Wagons**—Reversible labels are used by certain petroleum firms on tank wagons forwarded by rail. These wagons do not require over-labelling with railway labels. Consignees will reverse the labels after unloading.
 These labels must not be removed from the tanks by railway staff.
- (f) **H.M. Forces Traffic**—Special Navy, Army and R.A.F. wagon labels are used for wagons of traffic forwarded by these Services. The labels will be completed by the senders. Wagons with these labels do not require over-labelling with railway labels.
- (g) **Shipment Traffic**—The labels or targets of full wagon loads must show the name of the ship. (Not applicable to traffic for cross-channel or coastwise services.)

LABELS FROM RECEIVED LOADED WAGONS.

141. As soon as the load is discharged, all wagon labels must be taken off and retained as required for reference or returned to sending station for re-use as instructed locally. In the case of Continental Train Ferry Wagons, the label from the port of entry, Harwich or Dover or, alternatively, the "Special Vehicle Urgently Required" label (B.R.21223) should be detached but all Continental labels and R.I.V. labels must be left in position.

LABELLING OF CONTAINERS

142. Containers for shipment must be clearly labelled with the name and address of consignee and the gross weight, to facilitate handling at ports.

TRADERS' LABELS FOR WAGONS AND CONTAINERS

143. Traders are permitted to affix labels to wagons and containers provided they conform to the following :—

- (a) Dimensions—not to exceed 24 inches in length by 12 inches in depth, whether a cautionary notice (e.g. “Brittle goods”, “Shunt with care”, etc.) is included or not.
- (b) Wording—to be restricted to the trader’s name and address, commodities in which the trader deals, consignee’s name and address, and destination station.
- (c) Material and method of fixing :—
 - (i) **Wagons**—Cardboard, fixed by tacks or nails (which must not be driven through the wagon sheet) if no suitable clip or holder provided, or paper affixed by means of adhesive matter 1 inch wide applied along the four edges of the label. On plywood vans, only paper labels may be used; tacks or nails must not be used. On covered vans with sliding doors, the labels must not be fixed to that part of the wagon body over which the door passes when being opened or closed.
 - (ii) **Containers**—Only paper labels, affixed with adhesive matter 1 inch wide, applied along the four edges of the label, may be used; tacks or nails must not be used.

The labels must be fixed to the special boards where provided. Where boards are not provided, the labels must be fixed at the top right-hand corner of the side of large containers and at the bottom right-hand corner of the side of small containers.

- (d) To avoid confusion with railway “Not to go” or “Repairs” labels, the labels must not have red or green background.
 - (e) Labels must not obscure lettering or numbering on a wagon or container.
 - (f) Not more than one label to be placed on each side of a wagon or container.
144. Trader’s labels must be removed immediately on completion of the journey except where special instructions to the contrary have been given.

SHEETING AND UNSHEETING

EXAMINATION AND CARE OF SHEETS.

145. All sheets bear two dates, one stencilled in white (indicating the date of issue) and another in yellow. Sheets must not be used in traffic after the yellow date.

They must be carefully examined before use and also on removal from wagons, and any found defective or bearing a date in yellow which has been reached or passed must be properly folded, suitably chalked, and sent to the sheet factory.

146. New and redressed sheets, received from the sheet factory, must be opened immediately on receipt and put into use at once.
147. Sheets taken off wagons must be carefully folded as shown in the diagrams on page 36 so that when next used they will open right side outwards. They must be placed on end, under cover if possible, clear of rails and roadways, or removed to the appropriate store, where they should be stacked on end.

LOADING SHEETS INTO WAGONS.

148. Wagons used for loading sheets to factory or for transfer to another station must be swept clean and any projecting nails must be removed.

The sheets must be properly folded and stacked in the wagon on their ends and the load must be sheeted.

The wagon must be labelled with the "Folded Sheets" label (B.R.21212).

LOADS TO BE SHEETED.

149. All loads of goods and empties liable to damage by wet or fire, conveyed in open wagons, should be sheeted.

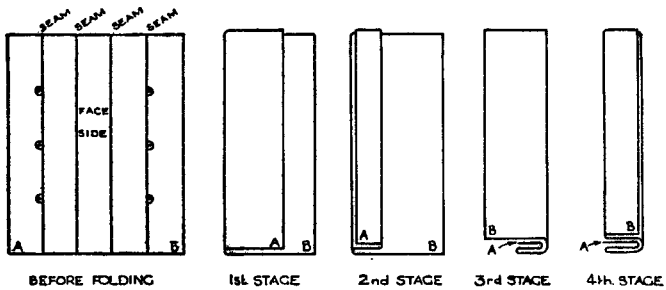
When surplus ropes are conveyed in open wagons, the vehicle must always be sheeted to prevent the contents becoming wet.

Discretion must be used as to the sheeting in dry weather of open trucks containing traffic such as pipes, etc., packed with heather or other material which will easily catch fire.

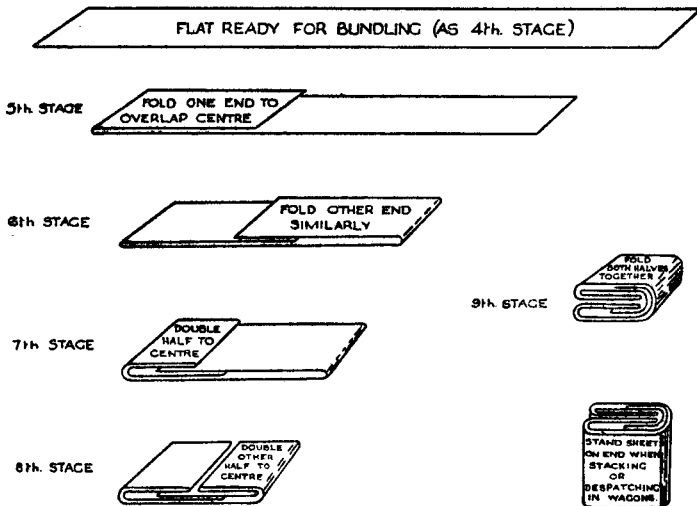
150. Loads must, whenever possible, be raised in the centre so that when the sheets are stretched tightly over the wagons and secured, the rain will run off. When a load cannot be sufficiently raised, a sheet supporter or loose tilt, if available, must be used. If a sheet supporter is not available, the goods must be covered by a sheet inside the wagon, placed over the load and tucked in at the sides and ends, and the wagon then sheeted in the normal way.

DIAGRAM SHOWING METHOD OF FOLDING AND BUNDLING
A WAGON SHEET.

FOLDING



BUNDLING



SHEETING WAGONS.

151. EVERY EFFORT MUST BE MADE TO AVOID HOLLOW SHEETING.
152. Care must be taken to avoid treading on damageable articles.
153. Sheets must be placed over the wagon the right side up, so that water will drain off the seams. They must be securely fastened by all their strings or ties, to the pins, hooks, etc., provided for the purpose, so that they are held taut and no gaps are left through which sparks, rain or water draining from the sheet, can enter.

Sheets must NOT be fastened to brake handles, springs, buffer plungers, couplings, vacuum pipes, or end door cotter rings.

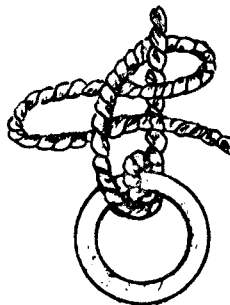
Open Merchandise wagons with standard fitments.

Sheet cleats (or rosettes) are provided, 5 at each side and 5 at each end, 2 $\frac{3}{4}$ " diameter so that the sheet tie may be wound round and gripped in the "V" of the rosette, thereby avoiding tying or cutting.

(N.B. These wagons have separate 2 pronged cleats 11" overall, for lashing ropes—Instruction 99).

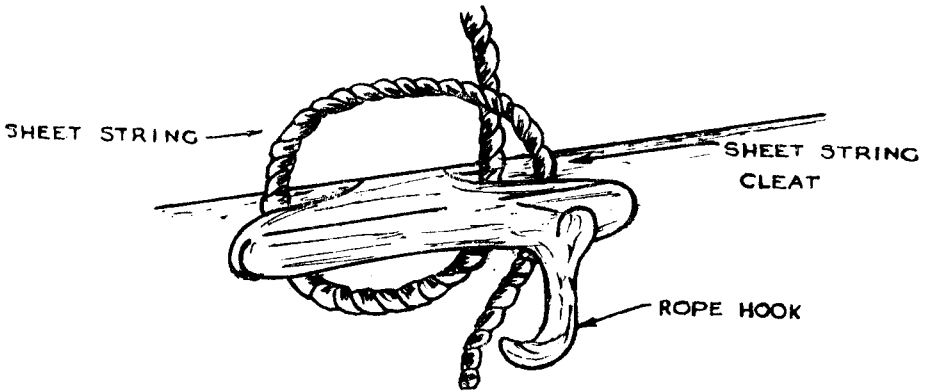
In the case of wagons still having some of the older fitments the following diagrams show how the sheet strings can be secured and easily released without cutting.

When the wagon is fitted with rings.



PULL TO UNFASTEN

When the wagon is fitted with combined sheet string cleat and rope hook.



The string is placed round the fitting, without a knot, and pulled tight.

154. If any of the sheet strings are missing or unfit for use, they must be replaced by new ones.
155. If the underside of a sheet is wet, a good covering of straw must be placed on top of the load before the sheet is spread.
156. Sheets used to cover articles with sharp or projecting corners or edges must be protected with packing, as necessary, to prevent the sheets being torn. When this kind of traffic, e.g. battens, boards, cases, machinery, needs to be roped, the ropes should, whenever practicable, be placed under the sheets to minimise the risk of damage to them.
157. Care must always be taken to prevent sheets from being damaged or contaminated by acids or other chemicals.
158. Where, owing to the length of the load or wagon, two sheets have to be used, one overlapping the other, the undersheet must be folded back about a foot so that water, which may get in under the top sheet, will not reach the goods.

The overlap of the top sheet should be towards the rear of the train whenever possible, so as to prevent sparks, wind or rain getting underneath the sheet. If necessary, ropes should be used to secure both sheets and loads.

DOUBLE SHEETING.

159. If it is not practicable to utilise covered vans for loading general merchandise, flour, sugar, meal, seeds, tea, flax and other traffic particularly susceptible to damage by wet and open wagons have to be used, these traffics should, during wet weather, be double sheeted. Both sheets must be taken over the wagon sides and ends and properly secured.

UNSHEETING WAGONS.

160. Care must be taken so that damage is not caused by :—
 - Treading on damageable articles;
 - Pulling sheets off roughly and dislodging goods;
 - Allowing water to run off sheets on to goods.
161. When wagons are received with water or snow on hollow sheets, it must be removed at **once** by scoops, siphoning, or other suitable means and not allowed to remain until the wagons are unloaded.
162. **SHEET STRINGS MUST BE UNTIED, NOT CUT AWAY FROM THE SHEET CLEAT OR RING.**
163. Sheets received on loaded wagons must be promptly released and recorded on the appropriate daily freight rolling stock return. “Spare” sheets must not be retained in expectation of traffic.
164. When sheets have been used to cover traffic of an objectionable nature, such as wet skins, etc., and are liable to taint other traffic, they should be thoroughly cleaned before re-use.
165. When sheets are received which have suffered exceptional damage, not the result of general usage, your supervisor must be informed.

USE OF SHEETS AT STATIONS.

166. Wagon sheets are for covering goods handed to the railway for transit. They must not be used by traders without authority. Except for limited periods when absolutely necessary, their use for covering goods in warehouses, or at wharves, is forbidden.
 - Wagon sheets must not be used on cartage vehicles.
167. A record must be taken of sheets sent to sidings, works and collieries situated away from the station and the places visited regularly and frequently to see that the sheets are correctly used and promptly returned to the station. Delay, damage and misuse whilst at sidings, works, etc., must be reported.
168. Station sheets are supplied in special circumstances for covering goods in warehouses and on wharves.
 - They must not be sent away from stations to which they belong nor used for any purpose other than that for which supplied. These sheets must be sent to the sheet factory for overhaul as necessary.
169. Cartage sheets must not be used to cover rail wagons. If any are received, having been so used, they must be returned to the owning station immediately.

MANCHESTER SHIP CANAL SHEETS.

170. Sheets stencilled "Manchester Ship Canal Railways" and "M.S.C. Co." are non-common user and any station receiving them either in use or in otherwise empty wagons, must return them to the M.S.C. Co., Manchester Docks.

PROTECTION OF TRAFFIC IN STORE

171. Leaking roofs and gutters, faulty windows, or other defects must be reported immediately to your supervisor. Pending repair, arrangements must be made to protect any goods likely to be affected.
172. Goods liable to be affected by dampness should be stored away from doors and, when necessary, covered by station sheets.
Such goods stored in cellars or warehouses with concrete floors likely to be affected by damp, should be placed on decking or some other form of wooden stillage, or dunnage.
Consult your supervisor when there is doubt about the suitability of premises for the storage of any commodity.
173. Delivery of goods from store must be in the order of receipt, unless otherwise ordered by the owners.

