

**(Private and not for publication)**

**BR. 20425**

**BRITISH RAILWAYS**

**2**

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**INSTRUCTIONS FOR HANDLING  
AND  
LOADING SPECIFIED TRAFFICS**

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**British Transport Commission,  
Railway Clearing House,  
203 Eversholt Street,  
LONDON, N.W.1.**

**MAY, 1957**

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# INSTRUCTIONS FOR HANDLING AND LOADING SPECIFIED TRAFFICS

## FOREWORD

All traffic must be handled and loaded with care but if the risk of injury to staff, or of damage to the traffic, track and passing trains—with the possibility of serious accidents—is to be avoided, some traffic must have special attention.

The instructions in this booklet cover such traffic (other than long and projecting loads) most likely to be dealt with. They are based on experience and if followed will do much towards avoiding injury to the staff, preserving the safety of the line, and ensuring the satisfaction of the railways' customers.

These instructions are supplementary to the general instructions in Booklet No. 1. Instructions about long and projecting and otherwise exceptional loads are in Booklet No. 3.

**1. BICYCLES, PERAMBULATORS, ETC. IN CRATES.**

These should be carefully protected from other articles likely to cause damage through the open spaces of the crates.

**2. BONDED GOODS, i.e., GOODS UNDER CUSTOMS AND EXCISE LOCK OR SEAL.**

(a) Each wagon containing goods under Customs and Excise Lock or Seal must have the special labels in the following form attached to it at the sending station:—

BRITISH RAILWAYS B.R. 21205

**USE  
BLOCK  
LETTERS**

\_\_\_\_\_ 19\_\_

## BONDED GOODS UNDER CUSTOMS AND EXCISE LOCK OR SEAL

From \_\_\_\_\_

This wagon contains goods subject to the supervision of H. M. Customs & Excise and, except in case of emergency, such as fire, accident, or leakage, it must not be interfered with unless a Customs & Excise Officer is present.

TO _____		<b>3</b>	Section _____	
Via _____			_____	
Letter	Number	* Crates Traffic		
Wagon		Gross Weight	T	C
Container		Heaviest Lift		

\* If crane power is not necessary, write on this panel.

In such an emergency, or if transhipment of goods under Customs & Excise Lock or Seal is necessary owing to breakdown of wagon, a Customs & Excise Officer must be called by telegraph or telephone, immediate steps being taken in the meantime to stop waste.

Place in charge of a Police Officer or responsible Railway Official.

Names of persons present when fire or leakage is discovered must be recorded.

Consignee \_\_\_\_\_

(b) Bonded goods must have the following label attached to *each package* when loaded in wagons of mixed goods:—

### BONDED GOODS.

This package contains Bonded Goods, and in the event of leakage, or any damage being found, the package must be held in charge of a Police Officer or responsible Railway Official, and a Customs and Excise Officer summoned by telephone or telegraph, immediate steps being taken in the meantime to stop the leakage.

## 2. BONDED GOODS—*continued.*

- (c) Whenever a leakage or any damage is discovered, the directions given on the labels attached to the wagons or the packages must immediately be complied with. Failure to do so may prevent the railway from obtaining remission of duty.

If a Customs and Excise Officer is not available, contact must be made with the Railway Police or failing this with the local Constabulary.

The Agent or other person in charge must see that further loss of the goods is prevented and that no part is consumed.

The names of all persons present when the leakage or damage was discovered must be recorded so that, if required, their evidence may be obtained.

A report of the circumstances must be sent immediately to the District Commercial Officer.

## 3. BONES, FAT AND OFFAL TO GLUE WORKS.

Whenever possible, bones, bone waste, fat and offal should be loaded in Mineral wagons, preferably steel bodied and without through top bar.

## 4. BUTTER, BACON, LARD, ETC.

In hot weather the following conditions must be observed for this traffic:—

- (a) It must be loaded in covered wagons whenever possible. If open wagons have to be used it should not be loaded immediately under the wagon sheet as in such a position it is liable to be damaged by the heat of the sun.
- (b) The traffic must be dealt with under cover where practicable. When it has to be unloaded in the open, it must be placed under cover as soon as possible.
- (c) Loads must be sheeted during collection or before being sent out for delivery.

## 5. CABLE DRUMS—LOADED AND EMPTY.

### (i) *Handling.*

- (a) Drums of cable must always be rolled in the direction shown by the arrow painted on the flanges but such rolling should be kept to a minimum.
- (b) The best way to turn a cable drum is to roll it on to two well-greased thin plates or boards about two feet square. The plates can then be used as a turntable and the drum rotated to the desired direction. The practice of using an iron bar under the bolt head is extremely dangerous to the operator and detrimental to the drum.

5. CABLE DRUMS—LOADED AND EMPTY—continued.

(ii) *Cranage.*

- (a) The maximum size of steel spindle which can be accommodated in the spindle hole must always be used.

This will ensure the spindle is not over-stressed.

- (b) The following sizes of spindle hole and spindle are most commonly used:—

<i>Diameter of Spindle Hole</i>	<i>Diameter of Spindle</i>
ins.	ins.
$1\frac{5}{8}$	$1\frac{1}{2}$
$2\frac{1}{4}$	2
$3\frac{1}{4}$	3
$4\frac{1}{4}$	4

- (c) Drums must always be lifted singly.

- (d) The use of a beam, spreader or stretcher is preferable (see Diagrams 1 and 2 below) and the lifting chains should hang vertically and parallel to the flanges of the drum. If slings only are used these should be of such length as to avoid undue pressure on the flange of the drum. The clearance between chain and flange should not exceed 3 ins.

Diagram 1.

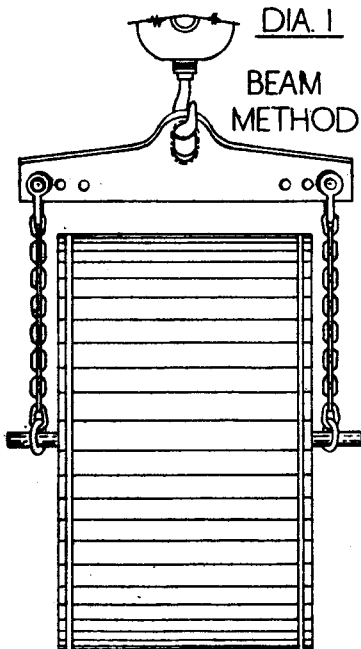
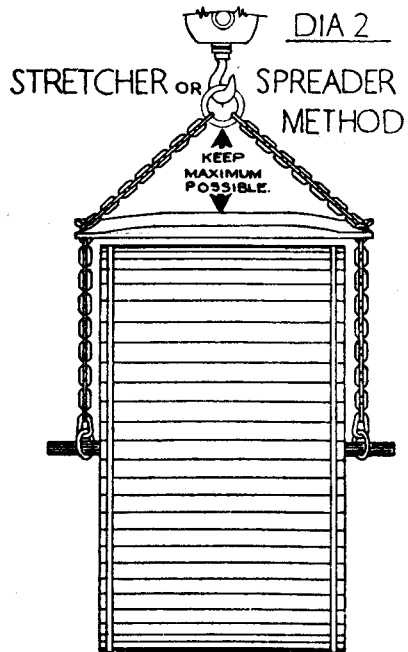


Diagram 2.



## 5. CABLE DRUMS—LOADED AND EMPTY—*continued.*

### (iii) *Storage of Drums of Cable.*

- (a) Store drums on well drained level hard ground, if possible on a concrete surface.
- (b) Where possible, drums should stand on battens (directly under the flange) with sufficient space to allow air circulation.
- (c) Never store a drum on its side or near any source of heat.

## 6. CARAVANS, SHOWMEN'S VANS ETC. (on their own wheels).

### (i) *Acceptance.*

Caravans, Showmen's Vans, and similar vehicles, on their own wheels, requiring the use of special appliances are not to be accepted for conveyance until it has been ascertained that there is accommodation at both the sending and receiving stations for dealing with such traffic.

They must be examined when brought to the station and senders must be requested to cover effectually any aperture, so as to prevent sparks entering the vehicles and causing fire.

### (ii) *Loading and Securing.*

Owing to the varied types of caravans and vans it is not possible to lay down a standard method of loading and securing but, as a general practice, the following arrangements are to be adopted for such vehicles, whether loaded or empty:—

- (a) Load on suitable wagons.
- (b) The wheels to be scotched (scotches to be nailed in position) and securely fastened to the rings or shackles on the wagon by ropes, straps or chains. The leading end of the vehicle body must be secured to each side of the wagon by ropes passed over the cross bar underneath the body, and tightly braced.

The wheels need not be scotched when the vehicles are loaded on carriage trucks.



## 6. CARAVANS, SHOWMEN'S VANS ETC. (on their own wheels) *cont.*

(ii)

- (c) Caravans with light springs are liable to oscillation during transit and, to prevent this, ropes must be used over the body of the caravan and adequately secured on each side of the wagon. Where, owing to the flimsy nature of the fabric, chafage is likely to occur, senders must be asked to supply bagging for placing between the caravan body and the securing ropes.

*Chains must never be used over the body of a caravan for securing purposes.*

- (d) Special care is necessary when dealing with vans, etc. on springs to ensure that the maximum load gauge is not exceeded, as when travelling empty, the height of such vehicles is greater than when travelling loaded.

## 7. CASKS, TINS, DRUMS, ETC.—RETURNED EMPTY.

(i) *Acceptance.*

- (a) Returned empty casks must be examined to ensure:—

That they are in sound condition, with vent pegs and bungs securely fixed.

The hoops are complete and the chimbs (ends) are unbroken.

If beer or vinegar casks, that they are not tainted by oil, creosote, etc.

- (b) If casks, tins, drums, etc. which have contained beer, oil, tar, varnish or other similar liquids are not properly bunged or otherwise closed and senders refuse to bung them, they should not be accepted until they have been examined by the foreman or other person in charge, who must refuse them if the quantity of dregs left in them might cause damage to other goods during transit.
- (c) Empties which have contained petroleum, paraffin oil or dangerous, corrosive or poisonous chemicals must not be accepted for conveyance unless in good condition, securely bunged or otherwise closed so as to be airtight. The special form of dangerous goods consignment note is required for such empties.
- (d) If casks (other than those referred to in (c) above) are damaged in any way when accepted, signature for them must be qualified and, in the case of beer casks, the destination station must be advised specially, the cask number being quoted and type of damage detailed.

## 7. CASKS, TINS, DRUMS, ETC.—RETURNED EMPTY—*continued.*

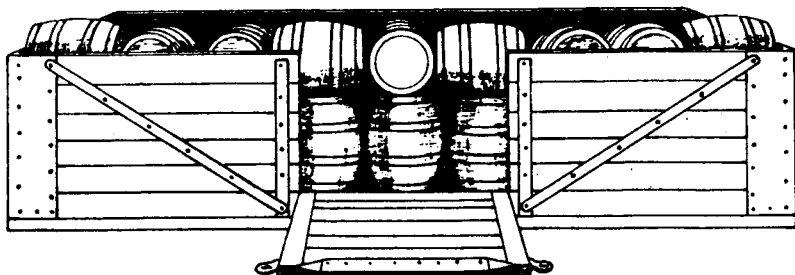
### (ii) *Loading of casks.*

*Note.*—Care must be taken to avoid contamination of beer and vinegar casks by oil, creosote, etc. as this would render them useless for refilling.

- (a) Returned empty casks must *not* be loaded end to end, either on their bilges or upright one tier upon another, as such loading may lead to the end of one cask knocking in the end of another.

Casks must *not* be dropped on or over the rave of the wagon. They must be carefully stowed by hand, the first tier being placed upright on the floor of the wagon and the second tier loaded on their bilges in such a position that they do not lie end to end (see diagram 1).

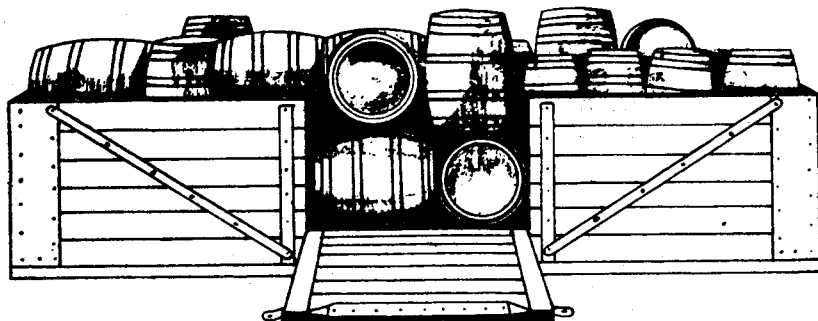
*Diagram 1.*



### BILGE TO BILGE AND/OR BILGE TO CHIMB.

An alternative method of loading casks of varying sizes is indicated in diagram 2:—

*Diagram 2.*



### BILGE TO BILGE AND/OR BILGE TO CHIMB.

All loads of these empties in open wagons must be made so that the top tier is sufficiently below the rave of the wagon to ensure safe transit.

## 8. CASTINGS (LIGHT).

- (a) These articles are extremely brittle and must not be dropped. When they cannot be safely dealt with by hand they must be lifted with rope slings, so placed as to balance the articles and not damage fragile parts. When not packed they should be carefully stowed and protected with straw or straw pads to prevent damage to them and other commodities loaded in the same wagon.
- (b) Electric and gas cookers, ranges, domestic and greenhouse boilers and similar articles should be barrowed upright with their backs resting on the barrow. They should not be "walked".  
Such articles protected by top and bottom boards bolted together, should be stowed lengthways in wagons with the securing bolts facing the end of the wagon.

When despatched unpacked, they must be stowed on their base lengthways, with the door facing the side of the wagon and well protected by straw pads or wads of twisted straw between each article and between the cookers, etc., and the ends and sides of the wagon.

- (c) Flanged pipes must be secured to the wagon end by chocks, "gates" or other suitable packing, and interlocking of the flanges avoided as far as possible. End door wagons must not be used.
- (d) Sewing machines not crated, in small consignments, should where practicable be loaded on their sides on the top of other traffic not liable to be damaged thereby, and packed with soft goods.

When it is necessary to load the machines on their feet they should be placed lengthwise, and held in position by goods reaching from the floor of the wagon *to above the level of the tables.*

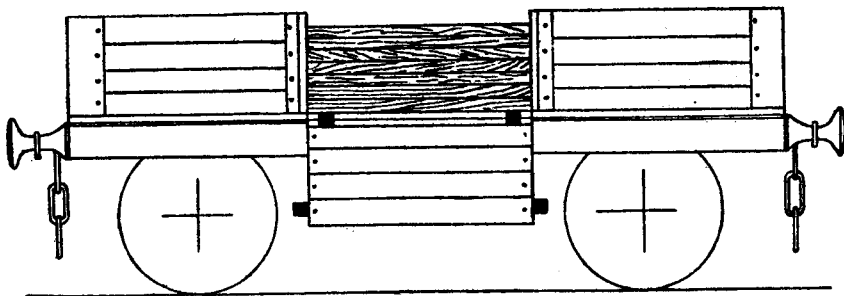
- (e) Broken or damaged castings being returned are to be considered of the same importance as new ones in carrying out these instructions.

## 9. DEALS, BOARDS, BATTENS, SCANTLINGS, ETC.

- (i) *Lengths of less than the inside length of the wagon.*

The traffic may be loaded in high-sided wagons by either of the methods shown in Diagrams 1 and 2:—

Diagram 1.

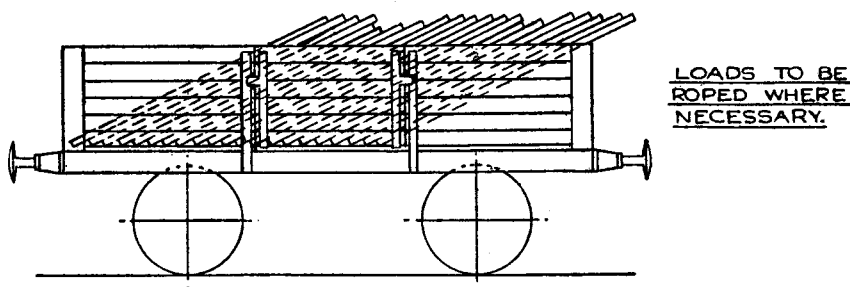


The load must be kept not less than 6 inches below the rave of the wagon.

## 9. DEALS, BOARDS, BATTENS, SCANTLINGS, ETC.—*continued.*

(i)

Diagram 2.



*Note.*—The overhang must not infringe the Instructions in Booklet No. 3, “Loading and securing of long and projecting and otherwise exceptional loads”.

(ii) *Lengths greater than the inside length of the wagon.*

Timber of lengths greater than the inside length of the wagon must be loaded according to the methods laid down in Booklet No. 3. “Instructions for the loading and securing of long and projecting and otherwise exceptional loads”.

## 10. EARTHENWARE, CHINA, ETC.

Earthenware and china is conveyed in withe crates, steel welded mesh crates, patent batten skeleton crates, casks, cartons and tea chests. They must be specially examined when received from sender. If there is any evidence of damage, details must be endorsed on the consignment note and a qualified signature given to the sender.

(i) *Lifting.*

(a) All suitable types must be lifted by crane when available and conditions permit. When lifted by crane, proper tackle must be used that will prevent pressure on the crate.

The most suitable lifting tackle is the “Lifting Beam and Crate Hooks”, failing this appliance suitable chains with crate hooks should be used, but a wooden stretcher must be used to prevent squeezing the crate. Chains must be not less than 6 feet in length. Grabs must not be used.

Full-faced or other heavy crates must be lifted with tackle which provides four lifting points.

## 10. EARTHENWARE, CHINA, ETC—*continued.*

(i)

- (b) Patent batten skeleton crates must be lifted with rope slings. This equipment must NOT be used for lifting any other type of crate.
- (c) Generally, the crates, other than patent batten skeleton crates, must be lifted in the following manner:—

	<i>Withe crates</i>	<i>Steel mesh crates</i>
Light and medium weight crates.	Top bars (first withe if crate weak or faulty).	Top bars.
Full-faced and heavy crates.	First withe.	Top bars.

When crates are received in wagons loaded on their sides the following methods of lifting should be adopted:—

	<i>Withe crates</i>	<i>Steel mesh crates</i>
Light and medium weight crates.	Top bar and bottom withe.	Bars forming the top of the crate, as loaded.

Full-faced and heavy crates must not be loaded on their sides.

- (d) Crates must always be lifted evenly and clear of the wagon sides when they are being loaded or discharged, the wagons being placed immediately opposite the crane.

(ii) *Handling.*

- (a) Standard shed hand-barrows may be used for small or light crates but care must be taken that the nose blade of the barrow does not enter the crate. The nose blade must not be used as a lever.

For heavier crates, special earthenware crate barrows or 4-wheeled low flat trolleys must be used.

- (b) Crates must not be dragged and crowbars and similar implements must not be used as levers in handling operations. Twisting must be avoided. Rollers must be used, when necessary, to get the crate into position in wagons or on road vehicles. The rollers must be wider than the base of the crate.
- (c) Casks of china and earthenware must not be rolled over uneven decks or other irregular surfaces.

## 10. EARTHENWARE, CHINA, ETC.—*continued.*

### (iii) *Loading.*

- (a) Crates of earthenware and china, etc., must, as far as possible, be loaded in open wagons. Crates must never be loaded overhanging the sides of wagons nor on end. Full-faced and heavy crates must be loaded face upwards. Articles or packages of any description must not be loaded on top of full-faced crates and heavy crates.
- (b) For full wagon loads, crates (except full-faced and heavy crates) may be loaded on their sides, bottom to bottom, on the floor of the wagon, the open face of each crate being towards the side of the wagon.

As a second tier, small and medium sized crates may be loaded bottom downwards to rest on the framework of the crates forming the first tier. Whenever practicable the heavier crates must be loaded on the wagon floor.

- (c) Full-faced crates and/or heavy crates must not be tiered, but loaded on the wagon floor on their bottoms.
- (d) During loading, sheeting, unsheeting, etc. of earthenware, china, etc., staff must not walk on the sheeted load or contents of the wagon.

### (iv) *Sheeting.*

All open wagons loaded with earthenware, china, etc. must be sheeted.

### (v) *Storage of Crates.*

- (a) Crates, other than full-faced, may be stored on end if it will facilitate subsequent handling or loading, but care is to be taken, in raising the crates to the upright position, that they are steadied so that the weight during the handling bears directly on the backbones.
- (b) Full-faced crates must be stored on their bottoms.

## 11. EGGS IN CASES OR CARTONS.

### (i) *Handling.*

- (a) Cases and cartons of eggs must always be handled with special care. They must be kept flat and right side up during every process of handling and must not be "walked" on their ends, turned on their sides, up-ended nor allowed to drop even the smallest distance. Cases must be handled by two men, one at each end.

When handling eggs packed in fibreboard cartons fitted with handgrips, the cartons must be lifted bodily if the handgrips show any signs of damage.

## 11. EGGS IN CASES OR CARTONS—*continued.*

(i)

- (b) When it is necessary to barrow cases or cartons of eggs, they must be lifted flat, by hand, on to and off barrows. Only barrows on which the packages can be kept level must be used.

(ii) *Loading.*

- (a) The strength and make of cases must be considered when deciding how many tiers high they can be safely loaded.

Fibreboard cartons must not be loaded more than four tiers high, with their edges flush to each other.

- (b) Full or part loads must be bedded with a heavy layer of mush (a mixture of wood shavings, chips, sawdust, etc.) or straw, to prevent heavy vibration during transit.
- (c) Small lots loaded in wagons with other goods must be loaded crosswise and goods of heavy weight must not be loaded on top.
- (d) When loading mixed loads of cases and cartons of eggs, the cartons must be loaded on top of the cases.
- (e) When loading cartage vehicles, vehicles with pneumatic tyres are to be used as far as practicable. The cases or cartons must not be allowed to rest on the raves of the vehicles.
- (f) The cases or cartons must not be walked on.

(iii) *Labelling.*

Wagons containing eggs are to be labelled "Shunt with care" in addition to the ordinary wagon label.

(iv) *Empties.*

When egg boxes are returned empty they must be loaded in covered vans or sheeted wagons, to prevent damage by wet to the cardboard or papier-mache fillings.

## 12. ESPARTO, MOSS LITTER, PULP, HEMP, ETC.

When wagons of these and similar traffics are loaded higher than the sides of the wagons, the loads must be securely roped.

All loads should be sheeted.

## 13. EXPLOSIVES AND OTHER DANGEROUS GOODS.

(A) *General Instructions.*

- (i) The acceptance and forwarding of explosives, inflammable liquids, dangerous, corrosive and poisonous chemicals, and other dangerous goods are subject to the regulations set out in the Dangerous Goods Section of the General Classification of Merchandise and in the Orange Booklet.

### 13. EXPLOSIVES AND OTHER DANGEROUS GOODS—*continued.*

- (A) (i) Empties which have contained certain commodities are subject to the same consigning and labelling regulations as when full.

The instructions apply equally to such traffics when forwarded from private sidings and a check must be made to ensure that the regulations and special instructions for loading and labelling wagons are carried out by senders.

- (ii) All consignments must be packed and labelled in accordance with the regulations. They must be consigned and described, in full classification terms, on the appropriate consignment notes unless a general agreement, authorised by Headquarters, is held.
- (iii) Casks, drums or other receptacles which are unsound or leaking must not be accepted for conveyance.
- (iv) Explosives and other dangerous goods must not be stored in any of British Railways enclosed sheds or warehouses without special authority from the District Commercial Officer, except where this is permitted by the regulations of the Classification of Dangerous Goods, e.g., certain packages of dangerous, corrosive and poisonous chemicals included in Section 2 thereof.
- (v) Traffic must be delivered as soon as possible after arrival or a messenger sent to the consignee with an advice note of arrival and a signature for the advice obtained. The exact time of delivery of the advice note must be recorded and consignees must be urged to remove such traffic at the very earliest moment.
- (vi) If there is any doubt about the acceptance, conveyance or disposal of any of the traffics referred to in the Dangerous Goods Section of the Classification, the District Commercial Officer should be consulted.
- (vii) All accidents and damage caused by or involving dangerous goods, explosives or inflammable liquids should be reported immediately to the Area Chemist and to the District Commercial Officer.

#### (B) **Explosives, Highly Inflammable and Inflammable Goods.**

- (i) Full loads of explosives and highly inflammable goods may only be dealt with *outside* railway warehouses or sheds and, as far as practicable, wagons are to be isolated before being loaded, or immediately on arrival for unloading.

Tank wagons must not be filled with, or emptied of, highly inflammable liquids at railway premises, except by special agreement in writing.

Open wagons conveying explosives must be sheeted.

Highly inflammable liquids must not be sheeted nor loaded in covered vehicles except in the case of small consignments loaded with other goods.

Wagons must be labelled with the following special labels, according to the class to which the goods belong:—

EXPLOSIVES  
HIGHLY INFLAMMABLE  
INFLAMMABLE.



13. **EXPLOSIVES AND OTHER DANGEROUS GOODS—continued.**

- (B) (ii) Small lots, when loaded with other goods, may be dealt with inside warehouses or sheds but must be handled, loaded and unloaded with great care, and immediately removed from such buildings.
- (iii) Magazine boots are provided, *and must be worn in gunpowder vans* by all men engaged in loading or unloading explosives. If magazine boots should not be available at a station required to load or unload such traffic, application must be made by telephone or telegraph to the District Commercial Officer for them to be supplied. The van must not be loaded or unloaded until the magazine boots are received and when the boots are no longer required they must be returned to the depot from which received, and not used for any other purpose.
- (iv) All gunpowder vans must be locked by forwarding station. These vans are fitted with a standard lock or padlock and it is not necessary to send the keys to the receiving station. At stations where gunpowder, etc. is dealt with regularly, keys are provided which will lock or unlock any gunpowder van. Additional keys are in possession of District Officers. If a consignment of explosives arrives at a station not provided with a key, application must be made by telephone or telegraph to the District Commercial Officer for a key to be supplied.
- (v) When the standard lock fitted to a gunpowder van is found to be defective and the van is not in need of other repairs, the van must *not* be sent to a repair depot, but application for a new lock is to be made to the nearest Carriage and Wagon Depot.

A man will be sent to fit a new lock.

(C) **Dangerous, Corrosive and Poisonous Chemicals.**

- (i) Each package must be labelled by senders with the special label prescribed for the traffic, as laid down in the Dangerous Goods Section of the General Classification of Merchandise.
- (ii) The following loading instructions must be strictly observed when traffics bear the labels shown:—

*Traffic  
labelled.*

“A”

*Loading instructions.*

This traffic and empties which have contained it, *must not in any circumstances be loaded in the same wagon with foodstuffs or foodstuff empties, unless placed in one of the special containers provided by the Railways.*

Further, *it is undesirable that this traffic, and empties which have contained it, should be loaded in the same wagon with explosives, inflammable liquids or other goods likely to be damaged in the event of leakage. If no other wagon is available, however, it may be loaded with such articles provided it be stowed far apart from them.*

### 13. EXPLOSIVES AND OTHER DANGEROUS GOODS—*continued.*

#### (C) (ii)

##### *Traffic labelled.*

##### *Loading Instructions.*

- “B” It is *undesirable* that this traffic should be loaded in the same wagon with *foodstuffs, foodstuff empties, explosives, inflammable liquids or other goods likely to be damaged* in the event of leakage. If another wagon is not available, however, it may be loaded with such articles provided it be stowed far apart from them.
- “C” This traffic, and empties which have contained it, *must not in any circumstances be loaded* in the same wagon with *foodstuffs, foodstuff empties, textile fabrics, paper goods, nor with any merchandise in paper, wood or fibre-board packages*. It should be dealt with in the open.
- Even the slightest vapours arising from packages containing Aniline Oil, Aniline Salt and kindred bodies, have a strong staining effect upon goods such as above-mentioned, as well as upon the cases containing them, if they are anywhere near. The use of special containers is not a sufficient protection against such dangers.
- “D” This traffic *must not be loaded* in the same wagon with *acids*.
- “E” This traffic *must not be loaded* in the same wagon with *glass or glassware*.
- “F” It is *undesirable* that this traffic should be loaded in the same wagon with *textile fabrics, paper packages, or other goods likely to be damaged*, in the event of leakage. If no other wagon is available, however, it may be loaded with such articles provided it be stowed far apart from them.

Small lots, when loaded with other goods, may be dealt with inside warehouses or sheds, but must be handled, loaded and unloaded with great care and immediately removed from such buildings.

Full loads of traffic must always be dealt with in the open.

These instructions also apply when traffic is loaded upon shed decks or cartage vehicles and care must be taken to see that no contamination or damage occurs to other goods in the vicinity.

- (iii) Wagons must be labelled with the following special labels, according to the class of traffic to which the goods belong:—

DANGEROUS GOODS  
“OCTEL” ANTI-KNOCK COMPOUND CONTAIN-  
ING TETRA ETHYL LEAD.  
SPENT OXIDE.

### 13. EXPLOSIVES, AND OTHER DANGEROUS GOODS—*continued.*

(C)

(iv) If there is damage to, or leakage of, Dangerous Goods the traffic, if loaded with other goods, must be unloaded and all other goods in the same wagon must be detained until it is determined that no dangerous contamination has occurred. A report to be sent immediately to the Area Chemist and to the District Commercial Officer.

(v) *Special Containers.*

Liquid-tight special containers are provided by the Railways to prevent the possibility of packages of chemicals placed in them from coming into contact with foodstuffs, etc., loaded in the same wagon.

These special containers, unless lined with lead or other acid-resisting material, must not be used for carboys containing the following liquids:—

Cleansing and washing fluids, acid.

Dipping acid.

Hydrochloric acid.

Nitrate of Iron solution.

Nitric acid.

Nitric and Hydrochloric acids, mixed (“Aqua Regia”).

Perchloride of Iron, liquid.

Sulphuric acid.

Sulphuric acid, diluted.

(vi) *Liquids in Carboys.*

(a) *Full wagon loads*—For traffic in full truck SINGLE TIER loads, the carboys must be placed close up to the ends and sides of the wagon and fastened together at all points of contact with strong cord or wire (to be provided by the Senders); any space must be either battened off with substantial timber positioned close to the shoulder of the carboys, or filled with other approved packing such as straw, sacks of straw, old hampers etc., or alternatively, a framework approved by British Railways may be fitted in the truck to support each carboy separately. The packing material or framework to be provided by the senders.

In the case of DOUBLE TIER loads, the carboy hampers in each tier must be fastened together at all points of contact with strong cord or wire (to be provided by the Senders). Where practicable, the end carboys in the top tier should be secured to the buffers or sheet hooks of the wagon. It is not desirable to use wagons the sides of which exceed the height of the load.

(b) *Less than full wagon loads*—Full and empty carboys and hampers must be packed by the senders in flat-bottomed boxes, crates or tubs, except when approved safety crates with standing rings are used. Carboys must be loaded in an upright position.

### 13. EXPLOSIVES, AND OTHER DANGEROUS GOODS—*continued.*

#### (C) (vi) (b)

The packages must be tied together and loaded at one end of the wagon where they will not damage other goods. They must be secured in position by a retaining bar or by roping them to the end of the wagon, or, where rings are provided in the wagon, by tying the packages to them.

Carboys must not be loaded on top of other goods or vice versa, nor in the same wagon with goods liable to be damaged if the carboys are broken or leakage of contents occurs.

#### (vii) "*Octel*" *Anti-knock Compound containing Tetra Ethyl Lead.*

This traffic is not normally handled by railway staff. The material is neither explosive nor inflammable at ordinary temperatures, but it is poisonous and it may enter the body either by absorption through the skin or by breathing the vapour. The compound is coloured orange or blue so that any leakage can be seen immediately. Furthermore it has a distinctive and rather sweet smell. If it can be smelled there is a dangerous concentration of the compound.

During transport the compound can only get into the body if leakage occurs from the containers. These are either tank wagons or specially constructed steel drums. The drums which are of great strength are sealed with an inner and outer bung. Rolling hoops are fitted as an added protection for the shell. The drums weigh approximately  $1\frac{1}{2}$  cwts. and 8 cwts. respectively.

Drums must be loaded "gunshot" on their rolling hoops, bungs uppermost and securely chocked. Stowage on ends is not permitted and no other commodity should be loaded in the same wagon. Irrespective of weight, drums must be conveyed through to destination in three plank drop-sided wagons, which must not be sheeted.

The tank wagons are also specially strongly constructed, the inner tank having thick covering of granulated cork with exterior sheeting of thick steel plate. The tank wagons do not have bottom outlet.

Wagons loaded with drums and tank wagons, containing "Octel", will bear wagon labels indicating the action to be taken in the event of leakage or spillage: the following are the measures which should be taken:—

- (1) Immediately notify and obtain advice from Associated Ethyl Co. Ltd. by telephone (Ellesmere Port 2381) or telegram (Ethylport, Ellesmere Port).
- (2) If the compound can be smelled, it is necessary to keep up wind unless respirator protection is available.
- (3) If the compound gets on the skin, wash immediately with paraffin; remove clothes if contaminated, then wash skin with soap and water. Contaminated clothes should be burnt.

13. EXPLOSIVES, AND OTHER DANGEROUS GOODS—*continued.*

**(D) Compressed and Liquefied Gases in Drums or Cylinders.**

- (i) Cylinders, whether covered or naked, and drums containing compressed or liquefied gases, must be carefully handled. They must not be loaded with inflammable liquids or empties which have contained inflammable liquids and must not be exposed to the heat of a fire, stove or other source of heat.
- (ii) Each container must be labelled by sender according to the regulations in the Dangerous Goods Section of the General Classification of Merchandise.
- (iii) Wagons containing the following traffics in large drums between 10 cwts. and 2 tons, or cylinders each weighing 11 cwts. or over, must be labelled with the standard Dangerous Goods label and, in addition, the special labels provided:—

LIQUEFIED ANHYDROUS AMMONIA.

LIQUEFIED CHLORINE.

LIQUEFIED METHYL CHLORIDE.

LIQUEFIED PHOSGENE.

The following additional conditions must be strictly carried out:—

- (a) The receptacles must be conveyed in through wagons to destination and not transhipped.
- (b) They must, so far as practicable, be dealt with entirely in the open.
- (c) They must be adequately secured and scotched to prevent rolling in the wagons.
- (d) The wagons must not contain:—
  - any corrosive or heavy articles likely to damage the receptacles;
  - any other articles likely to be damaged by Anhydrous Ammonia, Liquefied Chlorine, Methyl Chloride or Phosgene.
- (e) The wagons must be of types which allow the unloading to be performed without the receptacles at any time being lifted to a height exceeding 6 feet from the rail level.
- (f) When removing the receptacles from the vehicles, only cranes and hook chains which have been certified to have a lifting capacity of at least 10 *cwts. more than the gross weight of the receptacles* must be used. They must not be lifted by any other means than the special capacity can hooks and if these are not available the District Commercial Officer must be advised.

### 13. EXPLOSIVES, AND OTHER DANGEROUS GOODS—*continued.*

- (D) (iii) (g) If the receptacles are carted by the railway, the unloading at the consignee's premises must be performed entirely by, and at the responsibility of, the consignee.
- (h) Where it is found necessary to communicate with senders, as the result of emergency, every facility must be given to the firm's representative both at the point where the emergency occurs and *en route* to that point.
- (iv) Wagons containing LIQUID OXYGEN must be labelled with the special label for this traffic.

### 14. FRUIT.

- (a) Owing to its damageable nature, special care must be used when handling this traffic. Precautions must be taken to prevent packages falling off barrows, or the contents being spilled.
- (b) Casks, tubs, baskets or other receptacles, whether in full truck loads or loaded with other traffic, must be securely loaded so as to be kept upright during transit.
- (c) Fruit in open-topped packages must be loaded flat so that one package does not rest inside another and thus cause damage to the contents of the lower package. Tiers of packages must not be so high as to crush the bottom packages. Other goods must not be loaded on top. The load must be made solid and levelled so that the packages will not move or upset in transit.

### 15. FURNITURE.

#### (i) *Handling.*

Articles of furniture must be carried, whenever possible, rather than barrowed; they must never be "walked" on their corners or legs. Special care must be taken when getting them on or off road vehicles or barrows and in turning them into or out of wagons.

When unloading from road vehicles in shed or yard, the packages must not be dropped even the shortest distance but are to be carefully lowered until they touch the deck or ground.

#### (ii) *Loading.*

- (a) Furniture is easily damaged unless properly loaded and protected.

Covered wagons must be used for full loads.

- (b) When furniture is loaded with other goods it should, except for heavy pieces, be loaded on top of the load and straw or straw pads used liberally between packages and the side or end of the wagon, to prevent damage.
- (c) Bed ends, iron or wood, in bundles must be loaded on their sides, lengthwise. Small consignments of this traffic may be laid flat on top of the load.
- (d) Wardrobes or other long articles of furniture and all those fitted with glass panels or mirrors must be loaded lengthwise, with the glass panels or mirrors towards the side of the wagon and protected by suitable packing.
- (e) Furniture with leg projections is *not* to be loaded on the feet, or with the feet close to the end of the wagon, as this is liable to result in damage in transit.

## 15. FURNITURE—*continued.*

- (ii) (f) Chairs in bundles must always be placed upon their sides. If loaded otherwise they are liable to become strained and damaged.
- (g) Care must be taken to see that there is no pressure on the legs or other frail portions of the furniture and that there are no projections which might damage upholstered articles.
- (h) Furniture must be so loaded as to avoid projecting handles, legs, etc. damaging other traffic.

## 16. GALVANISED SHEETS, ETC.

Galvanised iron sheets, and similar goods with sharp edges or corners, are to be loaded so that they will not cause damage to drums of oil or paint or frail packages, by movement during transit. It is desirable to load iron sheets across the ends of wagons to minimise movement.

## 17. GAS PURIFYING REFUSE.

Gas Purifying Refuse, Green Copperas and other traffic of similar objectionable nature should only be loaded in Mineral wagons.

Ordinary wagons must not be used without authority.

## 18. GLASS (PLATE OR SHEET) IN CASES, CRATES OR CRIBS.

### (i) *Lifting by crane.*

- (a) In dealing with other than small packages of glass, lifting must be carried out by means of crane and lifting tackle in the following manner:—

<i>Type of package</i>	<i>Type of Lifting Tackle</i>	<i>How used</i>
Cases—Plate and Sheet.	Rope, wire, or double endless chain slings. *Glass or can hooks.	Slings to be placed round ends of cases under projecting top battens. Under top battens—one hook each side 6 inches each side of centre.
Large cases carried on special vehicles.	(a) Slings—wire or double endless chain capacity 2-tons. *(b) Glass or can hook.	Slings to be placed round end of cases under projecting battens. The actual positions to be determined by crane headroom. Under top battens one hook each side 6 inches each side of centre.

\* In no case may hooks with sharp points be used as these split and destroy the battens—which are specially designed to lift as indicated above.

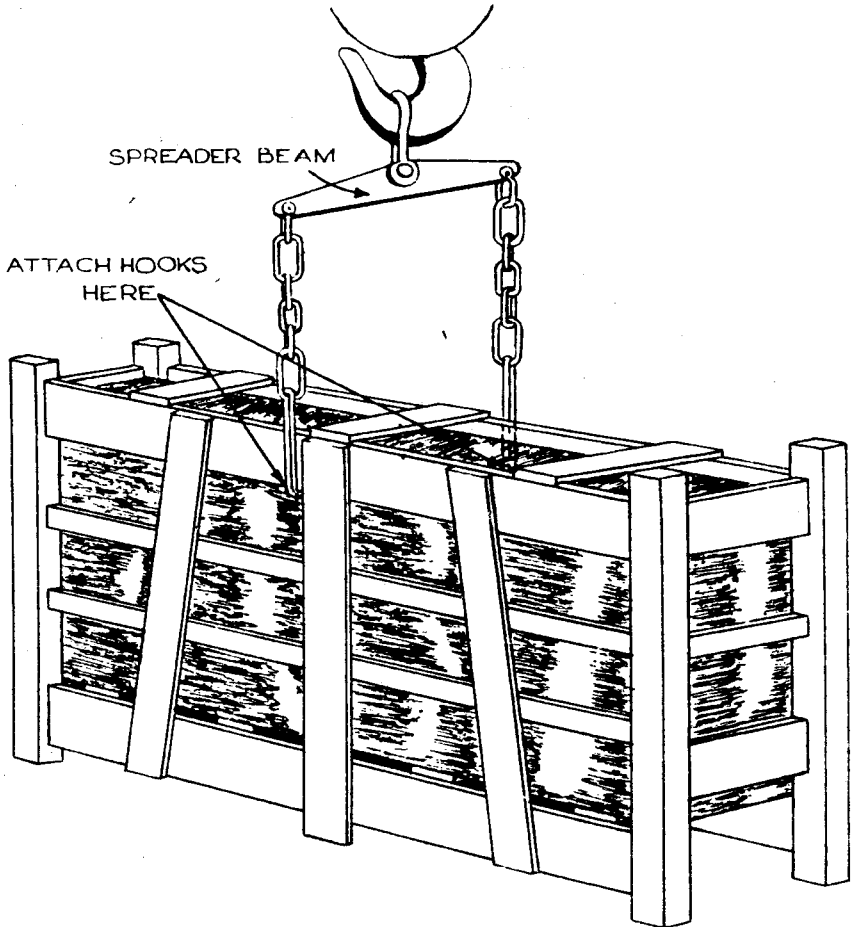
Method (a) to be adopted in preference to (b) particularly when unloading is performed in the open or by cranes with slewing jibs.

## 18. GLASS (PLATE OR SHEET) IN CASES, CRATES OR CRIBS

—continued.

- (i) Crates (wood or steel mesh) cribs. Spreader bar (1 foot between centres) fitted with glass hooks. Hooks to be placed as in diagram 1 below.

Diagram 1.



### METHOD OF ATTACHING GLASS HOOKS TO CRATES OR CRIBS FOR LIFTING.

- (b) Crates and cribs to be lifted into or out of wagons and road vehicles by the top side rails or bars which are specially strengthened for lifting purposes. Care must be taken to ensure an even balance and that the hooks are made secure before lifting is commenced. Each package must be lifted separately.



## 18. GLASS (PLATE OR SHEET) IN CASES, CRATES OR CRIBS

—continued.

- (i) (c) Wagons and road vehicles must be placed within the radius of the jib of the crane to ensure a vertical lift, without dragging. The packages must be kept clear of ends and sides of vehicles.

During windy weather special care must be taken to prevent the load swinging.

- (ii) *Lifting manually.*

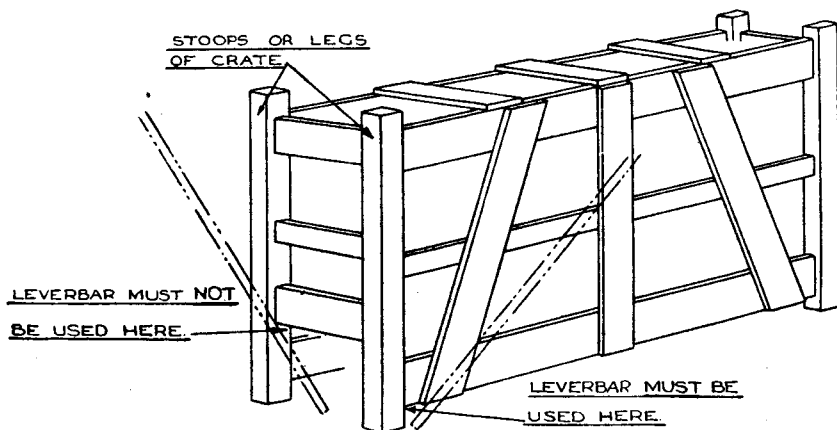
Where it is necessary to lift packages manually they must be raised bodily from each end to ensure a level lift, and every care must be exercised. There is great risk of damage to the contents of the packages, and of personal injury, if the lift is not made vertically.

- (iii) *Movement by lever bar.*

When crates fitted with “stoops” or legs have to be moved by means of leverbar, this must be applied under the bottom rail at the side near the “stoop”, where protection is provided (see diagram 2).

Packages of glass are not to be dragged or “walked”.

Diagram 2.



METHOD OF LEVERING CRATES BY LEVERBAR.

- (iv) *Barrowing.*

Crates, cribs and narrow cases less than 17 ins. in width must be moved on “Glass” wheels. Heavy cases wider than 17 ins. must be moved on “American” or other low bogie fitted trolleys, where available. When special appliances are not available, standard hand barrows may be used with *the blade protected by a straw pad or other suitable soft packing*. The total weight bearing on the hand barrow must be distributed over both cross (or side) rail members of the package (see diagram 3).

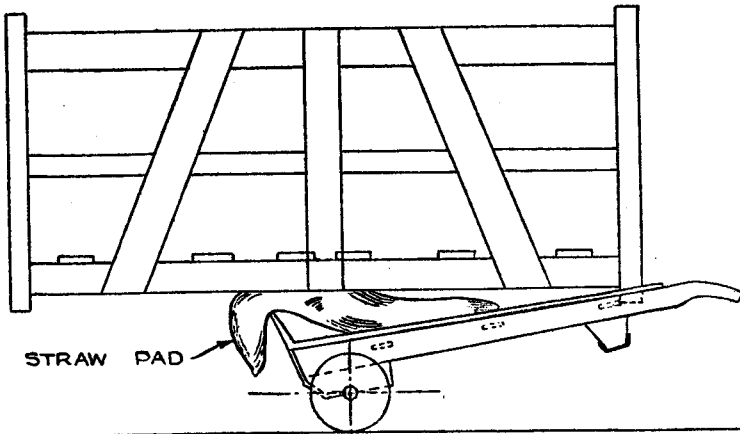
## 18. GLASS (PLATE OR SHEET) IN CASES, CRATES OR CRIBS

—continued.

- (iv) Crates or cribs must be kept upright and level when being barrowed and whenever possible they should be handled direct from road vehicle to wagon or vice versa. Each crate or crib must be dealt with separately.

When standard hand barrows are used a second man should be in front of the load to assist in keeping the crate balanced.

Diagram 3.



### (v) Loading and Securing.

- (a) Cases, crates and cribs must be loaded on their bases or feet lengthwise in the wagon with no space allowed for them to move during transit. **THEY MUST NEVER BE LAID FLAT.** Straw pads to be used between crates or cribs and wagon sides and ends as necessary. Scotchies must not be used for securing.

Packages must not be loaded on top of each other nor on top of other goods and other goods must not be loaded on top of them.

- (b) When packages of glass are loaded with other goods, they must be packed with goods which will not damage the glass, and secured by roping. Open wagons should be used whenever possible. Barrels or other articles which may roll or lean and heavy articles such as castings, bundles of iron, etc. must not be loaded against the packages.
- (c) When loading full loads of glass the following additional instructions must be followed:—

Only open wagons are to be used and those with dry floors should be selected.

Wagons with uneven floors must not be used.

Wagons with grease axle-boxes and/or bottom doors must not be used.

When possible, wagons selected should have sides lower than the height of the load so that roping can be efficiently performed.

## 18. GLASS (PLATE OR SHEET) IN CASES, CRATES OR CRIBS

—continued.

(v) (c) Approximately 50 per cent. of the height of the package must be below the rave of the wagon (maximum height of package 7 ft. 0 ins. with not more than two cases exceeding this up to gauge limits in the centre). Traffic of greater height must be loaded in glass wagons.

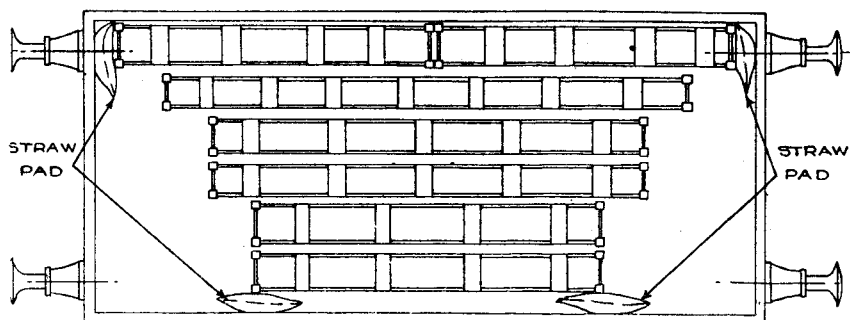
Wagons only to be loaded to capacity when two-thirds of wagon floor is covered. Cases to be loaded centrally over a length of 10 ft.

The above does not apply to empty cases which may be loaded to any height provided they are in gauge.

(d) When large cases, full or empty, are loaded on Glass wagons, the screw pads must be raised or lowered so that the pads at the end of the screws make contact with the thick lengthwise battens, not with the panelling of the cases.

*Method of loading and packing crates in wagons.*

*Diagram 4.*

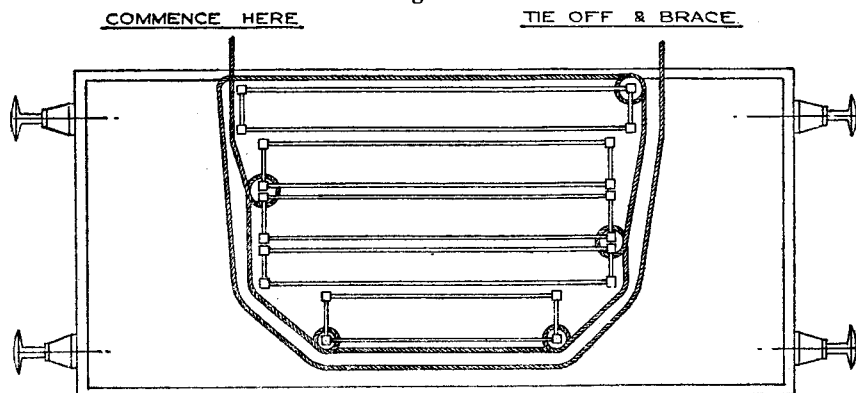


(Crates to be loaded lengthwise)

*Method of securing crates in wagons by rope.*

Crates and cribs which occupy the full width of the wagon, must be loaded lengthwise, midway between the ends of the wagon and roped firmly round and through end "stoop" of crates. The rope to be passed over the wagon side, braced and secured (see diagram 5).

*Diagram 5.*



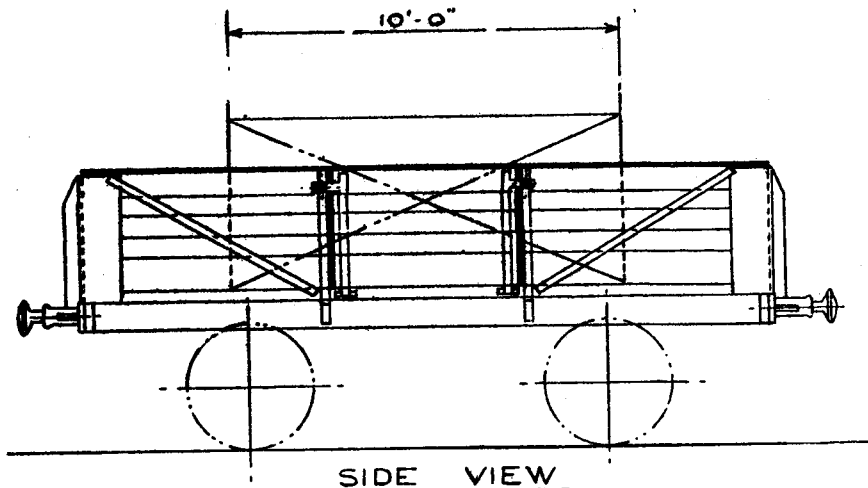
18. GLASS (PLATE OR SHEET) IN CASES, CRATES OR CRIBS

—continued.

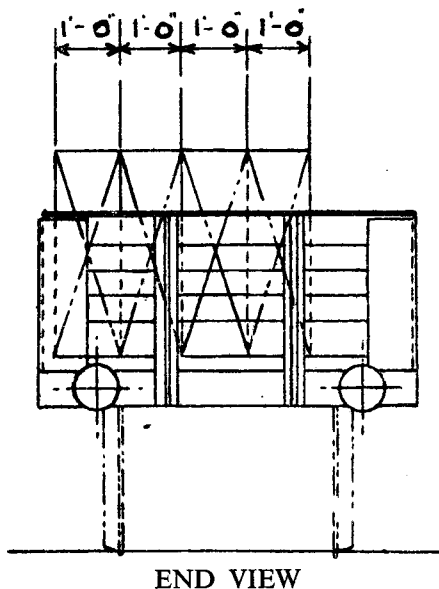
(v) *Part loads in 13 ton standard wagons.*

Where the full width of wagon is not occupied, loading (restricted to 15 cwts. per foot width over a length of 10 feet) must be to one side of the wagon, lengthwise, midway between the ends (see diagram 6). The load to be roped, as in diagram No. 5.

Diagram 6.



4 CASES—EACH  
WEIGHING 15 cwt.



## 18. GLASS (PLATE OR SHEET) IN CASES, CRATES OR CRIBS

—continued.

### (vi) *Sheeting.*

All wagons of glass must be sheeted. During sheeting and unsheeting the load must not be walked on.

### (vii) *Labelling.*

All wagons containing glass must be labelled with:—

(a) "Glass" labels or targets.

(b) "Shunt with care" labels.

### (viii) *Loading Cartage Vehicles.*

Where available, Glass Float or other suitable low loading vehicles must be used for all full vehicle loads or large crates, unless reception facilities at consignee's premises allow for direct unloading from ordinary vehicles.

The traffic must be loaded lengthwise and suitably packed with other goods to prevent toppling over, and the load roped.

The packages to be loaded to minimise handling during delivery.

Crates and cribs must be sheeted if there is risk of inclement weather.

### (ix) *Storage.*

If large crates or cribs require to be stored, they must be properly secured to prevent toppling over.

## 19. GLASSWARE, SILICA WARE, ELECTRODES, MARBLE SLABS, ETC.

Glassware, Silica ware and Electrodes (carbon sticks) are extremely brittle and require great care in handling. Cases should not be allowed to drop even the smallest distance and they must be loaded so that they cannot move during transit.

Marble slabs, whether loose or in crates, should be placed lengthwise on edge down the side of the wagon; when loose, straw packing to be used.

## 20. GRAIN AND FLOUR IN SACKS OR BAGS.

To ensure safe transit of wagons containing grain or flour in sacks or bags, the following instructions and diagrams are given as a guide to the proper methods of loading in standard high goods and mineral wagons. Low and medium wagons must not be used.

*Note.*—Owing to the varying sizes of sacks and bags, the diagram is not intended to show the number to be loaded in wagons but only indicate the method of stowing. The carrying capacity of the wagon must never be exceeded.

*Method of loading.*

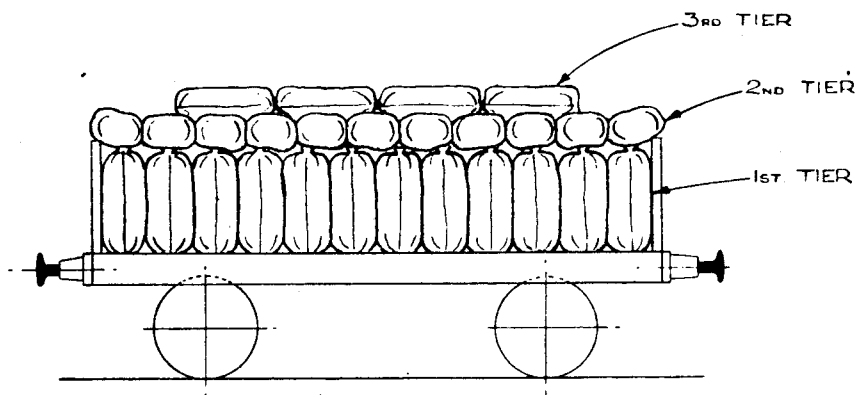
*First tier.*—The body of the wagon must be tightly packed with sacks or bags standing on end.

*Second tier.*—Two or three sacks or bags placed across the wagon at each end, mouths inwards, to form end supports for the remainder of the second tier. The intervening space is then filled with sacks or bags loaded across the wagon, mouth to mouth.

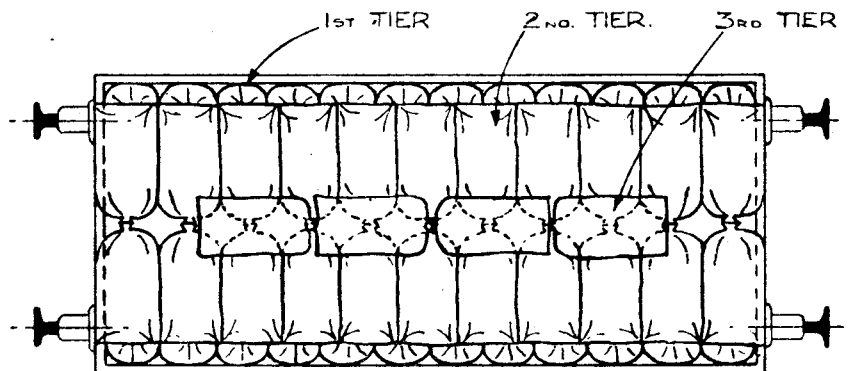
*Third tier.*—Three or four sacks or bags loaded lengthwise through the centre of the load, to form a tilt.

Loads must be sheeted and, where necessary, roped.

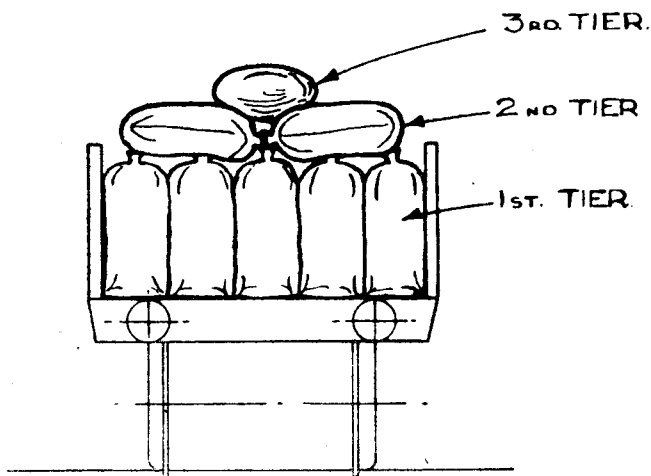
20. GRAIN AND FLOUR IN SACKS OR BAGS—continued.



SECTION



PLAN VIEW



CROSS SECTION

## 21. HAY AND STRAW.

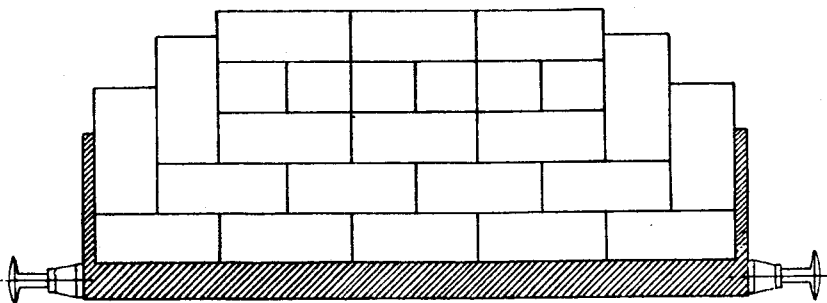
### (A) Machine Pressed Bales.

#### (i) *Loading.*

##### *Standard Method.*

Bales must be loaded in High Goods wagons with sides of five or more planks, in accordance with the following diagram:—

*Diagram 1.*



The diagram indicates the method of loading but, as wagons and bales vary in size, it is impracticable to lay down the number of bales to be loaded in a wagon.

The loading of some bales upright at the ends of the wagon is to prevent longitudinal movement in transit and this principle must always be followed. Every care must be taken to construct a compact load and keep the traffic within gauge.

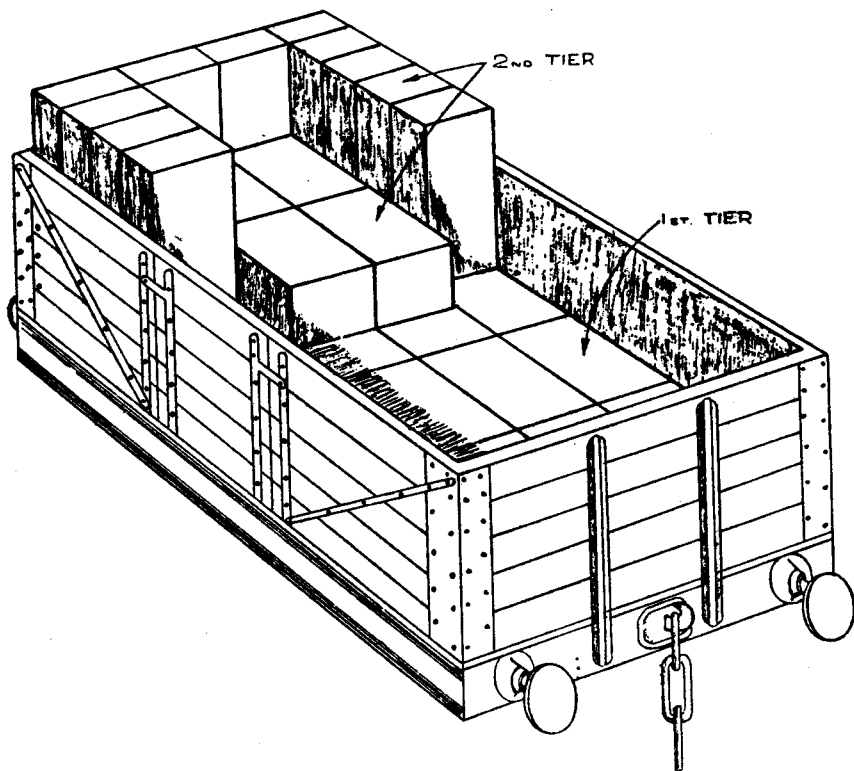
## 21. HAY AND STRAW—continued.

(A)

*Alternative method of loading without roping.*

The following method of making a compact load, with both end and side locking of bales, may be used when ropes are in short supply, to avoid traffic being held under load awaiting roping:—

Diagram 2 (Fig. 1.)



### *1st TIER*

20 BALES LAID FLAT, IN FIVE ROWS OF FOUR BALES ACROSS THE WAGON.

### *2nd TIER.*

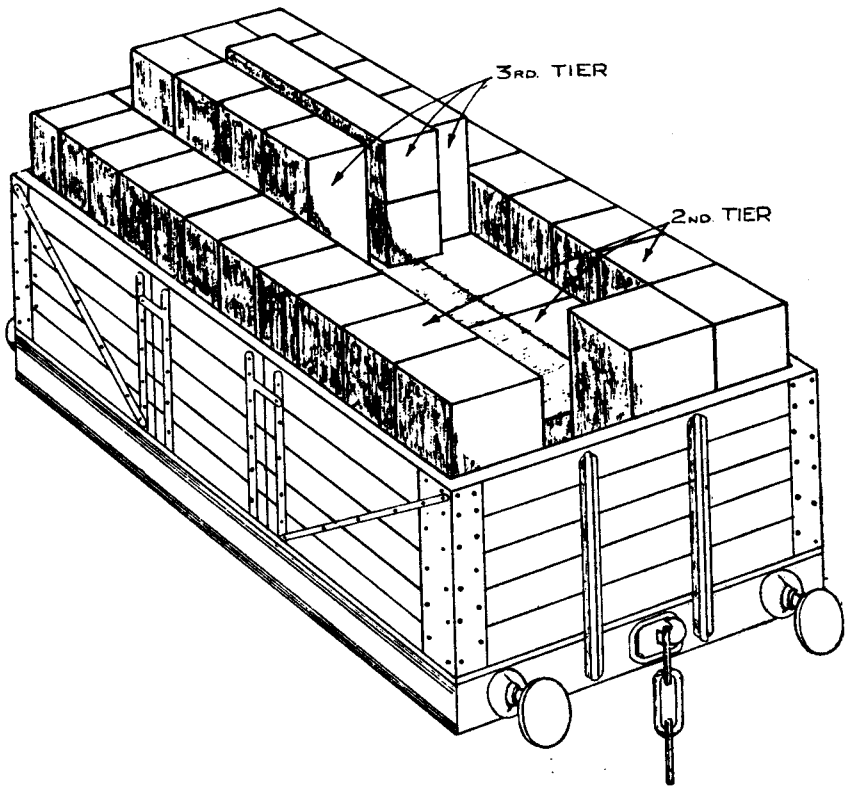
26 BALES STOOD ON END COMPLETELY ROUND THE SIDES AND ENDS OF THE WAGON TO FORM WALLS, WITH ONE TIER OF 8 BALES LAID FLAT IN THE CENTRE SPACE TO LOCK THE BOTTOM OF THE BALES FORMING THE WALLS.



21. HAY AND STRAW—continued.

(A) *Alternative method of loading without roping.*

Diagram 2 (Fig. 2).



*2nd TIER.*

26 BALES STOOD ON END COMPLETELY ROUND THE SIDES AND ENDS OF THE WAGON TO FORM WALLS, WITH ONE TIER OF 8 BALES LAID FLAT IN THE CENTRE SPACE TO LOCK THE BOTTOM OF THE BALES FORMING THE WALLS.

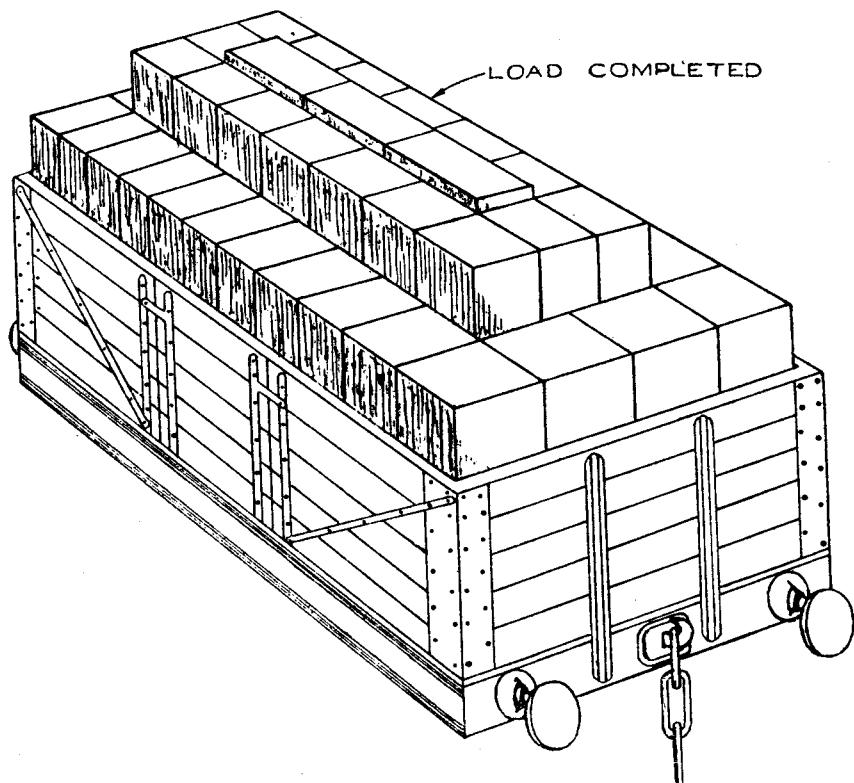
*3rd TIER.*

16 BALES STOOD ON END ON THE 8 BALES IN THE SECOND TIER PREVIOUSLY MENTIONED, TO FORM A FURTHER FOUR WALLS. CENTRE THEN FILLED WITH 6 BALES LAID FLAT IN 2 TIERS OF 3.

## 21. HAY AND STRAW—continued.

### (A) *Alternative method of loading without roping.*

Diagram 2 (Fig. 3).



#### (ii) *Sheeting.*

Loads must be completely covered by one or, if necessary, two sheets.

When two sheets are used they must overlap in the centre of the load with the undersheet folded back about one foot, and securely tied down.

#### (iii) *Roping.*

Wagons loaded by the standard method illustrated in diagram 1 must be roped by either of the methods indicated in diagrams 3 and 4 which follow.

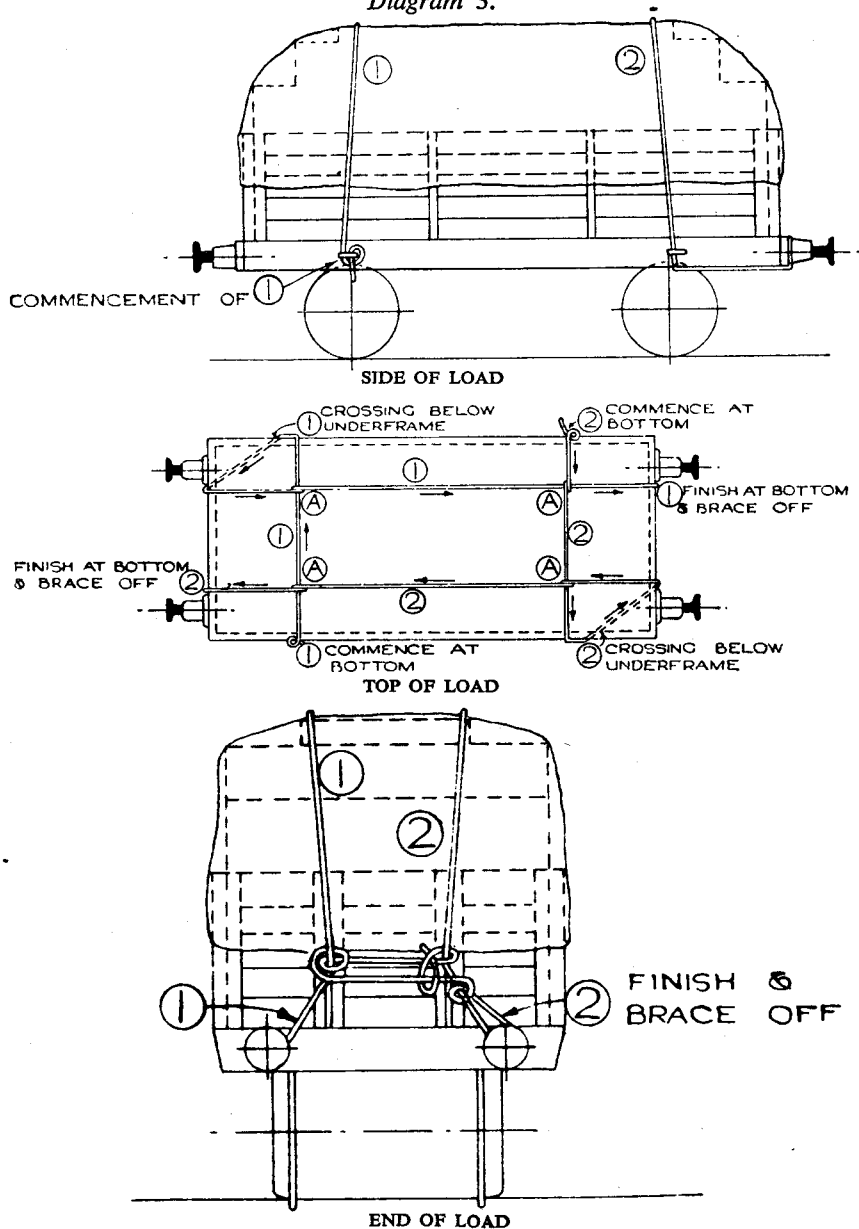
Dry ropes must be used and placed over the sheets.

With regard to the method illustrated in diagram 4, where sufficient ropes are available a third rope may be used across the centre of the load over the diagonal ropes.

21. HAY AND STRAW—continued.

Roping of Hay and Straw traffic loaded in high-sided wagons.

Diagram 3.



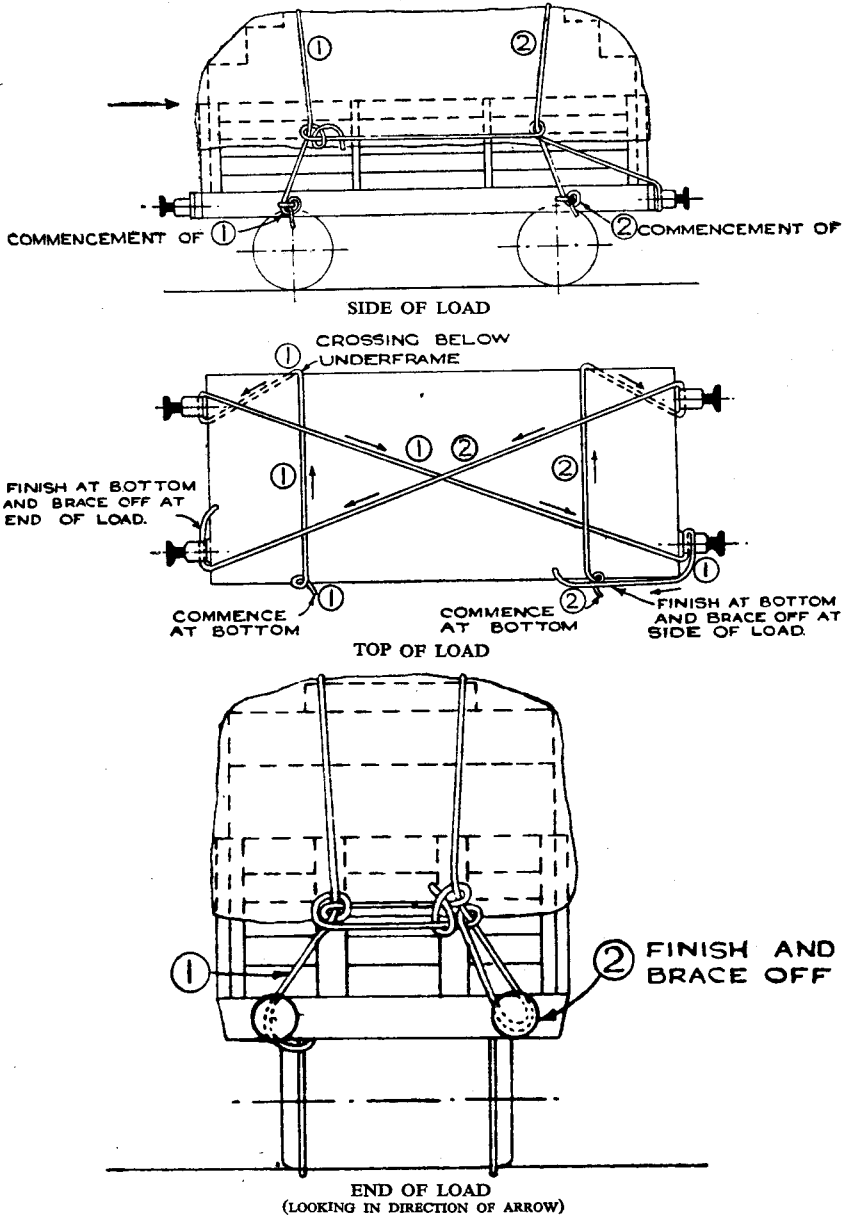
Note.

NOT LESS THAN TWO ROPES TO BE USED.  
 COURSE OF EACH ROPE INDICATED BY FIGURES 1 AND 2.  
 ROPES TO PASS ROUND EACH OTHER AS INDICATED BY LETTER A.

21. HAY AND STRAW—*continued.*

*Roping of Hay and Straw traffic loaded in high-sided wagons.*

Diagram 4.



**Note.**

2 ROPES USED.

COURSE OF EACH ROPE INDICATED BY FIGURES 1 AND 2.  
ROPES BRACED OFF AT ONE SIDE AND END AS SHOWN.

## 21. HAY AND STRAW—*continued.*

### (B) Hand Pressed Bales.

Hand pressed bales and trusses of hay and straw are not standard in size or shape and a strictly uniform method of loading cannot be laid down.

Staff concerned must take every precaution to ensure that good and safe loads are made, the following points being taken as broad principles:—

- (a) High goods wagons with sides of five or more planks must be used.
- (b) All bales must be loaded flat. The floor of the wagon must be firmly packed. Each alternate tier must be loaded in reverse directions. No portion of the bales must overhang the rave of the wagon.
- (c) Each tier above the wagon rave must be inset approximately 6 inches from the edge of the previous tier.
- (d) To prevent hollow sheeting, the final tier should consist of one or two rows of bales placed along the centre of the load.

*Sheeting and Roping* is the same as for Machine pressed bales.

### (C) Examination.

Every wagon of hay and straw must be carefully examined prior to despatch, by a competent person at the sending station and it must not be sent forward unless it is securely loaded and sheeted; also roped, except when loaded by the alternative method without roping, shown in diagram 2.

## 22. HIDES, SKINS AND PELTS—DRIED.

*Protection from wet.*

Dried hides and skins must be protected from wet during loading and unloading. If loaded in open wagons, they must be sheeted.

## 23. HIDES, SKINS AND PELTS—WET.

### (i) Handling.

When this traffic is handled there is a risk of contracting "Anthrax", i.e. a form of blood poisoning, unless the hands are protected.

At stations where such traffic is regularly dealt with, rubber gloves and oilskin aprons are supplied for use of staff handling the traffic. Supplies may be requisitioned by other stations, as required, through the District Commercial Officer.

The traffic should not be handled without these protections.

After use the gloves and aprons must be thoroughly cleansed and stored where they are easily available to staff requiring to use them.

### (ii) Loading.

- (a) Wagons and road vehicles in which this traffic is conveyed must be clean before being loaded.

Open wagons must be used for full loads and the traffic evenly distributed over the floors.

**23. HIDES, SKINS AND PELTS—WET—continued.**

- (b) When different lots are placed in the same wagon each consignment must be kept separate to avoid wrong deliveries.
- (c) When small consignments have to be loaded as part contents of a wagon they must be separated from other goods. They must be loaded as far apart as possible from injurious substances such as chemicals, paint, manure, iron and mineral ores. Sheep skins must not be allowed to contact wood chips.

(iii) *Sheeting.*

Open wagons must be sheeted.

(iv) *Protection during transit.*

The goods are not to be allowed to lie exposed unnecessarily to the atmosphere during any part of the transit, particularly during hot weather, and care must be taken that they are not damaged by contact with the ground.

**24. LEAD.**

(i) *Handling.*

This traffic must be handled with special care and lead hooks used for lifting rolls. Piping must not be rolled; it must be barrowed with the barrow blade covered.

(ii) *Loading.*

Rolls of lead must be kept clear of projections or nails in wagon floors. Small consignments of lead piping should be loaded along wagon sides and packed with baled or similar goods. Piping must not be allowed to come in contact with casks or large cases which may cause damage by denting and compressing it.

**25. LINOLEUM AND BACKED OILCLOTH.**

(i) *Acceptance.*

Each roll must be examined before acceptance. If any defects are found (particularly tears on outer coverings and dentings at ends) sender's attention must be drawn and a qualified signature given.

(ii) *Handling.*

The rolls must not be:—

- dropped,
  - handled with hand hooks,
  - rolled along the floor, or over the sides or ends of vehicles,
  - lowered from road vehicles to ground by gantries or skids,
- and the end cores of the rolls must not be removed.

Special care is necessary when dealing with rolls of exceptional length and/or weight, to avoid injury to staff and damage to the goods.

(iii) *Barrowing.*

- (a) Long rolls must be carefully raised on end (address label on top) and loaded lengthwise on a handbarrow, by slightly tilting the roll to allow the blade of the barrow to pass underneath the end of the roll. The balance of the roll must be maintained during barrowing by allowing the roll to rest on the barrower's shoulder.

## 25. LINOLEUM AND BACKED OILCLOTH—*continued.*

- (b) Short rolls may be loaded on hand barrows by placing one roll on end lengthwise and another across the top of the first.
  - (c) Where special protection for barrow blades (e.g., barrow nose boards), is available, it should be used.
  - (d) Rolls must not be permitted to rest on the blade edge of handbarrows. All rolls must be wheeled into the wagon or on to the cartage vehicle, carefully removed from the barrow, and positioned.
- (iv) *Loading of wagons.*
- (a) Covered wagons with floors clean, dry and free from protruding nails etc. should be used. Wood chippings, sawdust or straw to be used on floors to prevent chafing of the rolls. Straw pads must be placed at the doors and against the sides and ends of the wagons to protect the traffic from door fittings and internal bolt heads.
  - (b) When loading wagons direct from road vehicles alongside, rolls must be manually raised on end, address label uppermost, and lifted bodily into the wagon.
  - (c) Rolls more than six feet long must be loaded lengthwise but shorter rolls should be loaded crosswise.  
Loads may be made up to four tiers high, the second and subsequent tiers being nested in the saddles of the previous tier.
  - (d) Consignments of a few rolls only, loaded with other goods, must be protected by straw pads as necessary. They must not be loaded adjacent to or under awkward, greasy or oily consignments which may fracture or damage the rolls by contact.
- (v) *Loading of Road Vehicles.*
- (a) Floors of vehicles must be clean, dry and free from protruding nails, etc. If necessary, wood chippings, etc. to be used to protect the rolls from wet or damp floor boards.
  - (b) Rolls to be loaded lengthwise up to four tiers high. They must be so loaded that the address label can be readily seen thus avoiding the roll having to be turned whilst on the vehicle.
- (vi) *Storage in Warehouses.*
- (a) Rolls must be stored in a clean and dry position, on a floor free from projecting nails, etc., and away from other goods likely to damage them.
  - (b) Rolls in paper wrapping must be stowed on end with the address labels or marks visible, to avoid unnecessary handling.  
Rolls enclosed in canvas may be laid down, address labels or marks being exposed.
  - (c) If necessary, additional covering may be provided with linen sheets.

## 26. LIQUIDS—NON-DANGEROUS.

### (A) Wines and Spirits in Casks and Jars.

#### (i) General.

- (a) Owing to the very high value of this traffic, experienced men should be deputed to deal with it.
- (b) The bungs of casks loaded on the bilge must be uppermost. When casks are loaded on end, the bungs must be clear of adjacent casks and wagon sides.
- (c) Rope rings only must be used for scotching; bricks, stones or other hard substances must not be used for securing.
- (d) The roping of casks together inside a wagon without the rope being secured to the vehicle is prohibited.
- (e) In mixed loads, casks and jars must not be loaded near traffic especially liable to damage if leakage occurs or near casks containing creosote, paraffin or other oils.
- (f) "Shunt with Care" labels must always be used on wagons containing this traffic.
- (g) When consignments are conveyed under Customs and Excise Lock or Seal, the labelling instructions in Instruction No. 2 of this book must be carried out.
- (h) This traffic must not be sheeted, except when part loads are conveyed with other traffic which requires sheeting.

#### (ii) Tuns and Pipes.

Medium-sized wagons with full length drop sides to be used. Covered wagons must not be used, except for individual casks included in loads of sundries.

##### (a) Full loads.

Tuns and pipes are to be loaded on their bilges, lengthwise along both sides of the wagon, each being well-nested on a rope ring of suitable circumference and depth to prevent contact with the floor of the wagon.

They must be securely scotched and roped over the sides of the wagon to the rings or horsehooks on the sole bar, as well as over the end of the wagon to the buffer castings, each cask being double roped and braced. The drop-sides of the wagon must be put up and properly secured before roping takes place.

Straw pads are to be used to prevent chafage with the side of the wagon during transit.

##### (b) Mixed or part loads.

If in mixed loads in open wagons or in partly filled open wagons, tuns and pipes must be similarly loaded. Those in mixed loads to be securely packed with other goods to prevent shifting in transit.

#### (iii) Butts, Puncheons, Hogsheads, Tierces, etc.

Wagons with sides not less than 21 inches and at least six inches lower than the tops of the casks, to allow for roping, should be used.



26. **LIQUIDS—NON-DANGEROUS—continued.**

(A) (iii)

- (a) *Full loads.* (i.e. where wagon space will not allow any cask to fall over.)

Casks to be loaded on their ends and secured by ropes or tarpaulin slings. When securing, a load to be considered as two half loads, each half being secured over the wagon end to the buffer castings and braced. The casks should be held by tarpaulin slings or ropes hitched round at least one cask at the division of the load, additional ropes being used as necessary.

- (b) *Part loads.* (i.e. where wagon space would allow any cask to fall over.)

*With consignments of not more than five casks,* casks should be loaded on their ends at one end of the wagon and secured in position by ropes or tarpaulin slings hitched completely round the outer casks and to the the buffer castings.

Individual casks should be similarly secured in position if other traffic is not available to fill the wagon.

When individual casks containing Wines and Spirits cannot be secured by rope or tarpaulin slings to the buffer castings or are loaded in vans, they must be secured to prevent transit movement. It is not sufficient simply to load casks on end even when other traffic is available to fill the rail vehicle.

*With consignments of more than five casks,* the casks must be loaded on their ends, evenly distributed (as far as possible) at each end of the wagon, and secured as above.

(iv) **Jars.**

Except where special basket barrows are provided, jars must not be barrowed but carried to the wagon and loaded along the sides, well packed with straw and supported by other suitable goods.

(B) **Liquids (other than Wines and Spirits) in Casks and Drums.**

(For dangerous, corrosive and poisonous liquids see Instruction No. 13.)

- (a) All casks having a worn or bulged appearance must be loaded on the bilge, bung uppermost. Other casks and drums, should, as far as practicable, be loaded on end and secured as shown in clause A (iii) for wines and spirits, but when loaded on bilge bungs must be uppermost. When casks or drums are loaded on end the bung, if on the bilge, must be kept clear of adjacent casks or drums and the wagon sides. When the bung is on the end, this end must be uppermost.

- (b) When only high-sided wagons are available for part loads of this traffic and it is impracticable to secure barrels or drums by rope or tarpaulin sling or by packing with other traffic,

## 26. LIQUIDS—NON-DANGEROUS—*continued.*

### (B) (b)

- they may be loaded on the bilge lengthwise in the wagon and secured individually with scotches to prevent movement in transit and rolling out of the wagon when the doors are opened.
- (c) Casks or drums of chemicals which cannot be loaded on end owing to the nature of the contents must also be loaded on the bilge and properly secured.
  - (d) In mixed loads, casks and drums must be loaded away from traffic especially liable to damage if leakage occurs. Casks of ale or vinegar must not be loaded near casks of creosote, paraffin or other oils.
  - (e) Straw rings or pads, wads of twisted straw, or wood scotches may be used for securing and protection. Bricks, stones or other hard substances must not be used.
  - (f) Drums must not be loaded with more than one-third of their diameters, or one-third of their lengths if loaded upright, above the sides of the wagon. Those on the top layer which are next to the end of the wagon must be loaded lengthwise so that there is no risk of them rolling over the end of the wagon.
  - (g) Drums must not be dropped or damage may occur to the rims and cause leakage.

## 27. MACHINERY AND KINDRED TRAFFICS.

### (i) *Acceptance.*

- (a) All projecting or damageable parts should be cased or battened. If not, a "Damageable goods not properly protected by packing" consignment note must be signed by sender.
- (b) Senders should label the articles with any necessary special handling instructions and be requested to mark the best lifting points. Agents should maintain close contact with firms to ensure that this request is complied with whenever opportunity offers.
- (c) Articles requiring the use of special appliances, or of exceptional weight or bulk, must not be accepted for conveyance until it is known that there is accommodation at both sending and receiving stations for dealing with them.

### (ii) *Lifting.*

- (a) The correct lifting appliance of adequate capacity must be used.

The chains, slings, ropes, etc. must be securely fastened to the article but must not be in contact with projections or delicate parts of machinery.
- (b) Rope slings or double-chain slings must be used for crates and cases except—
  - when standard or special lifting tackle is necessary, or
  - when lugs, rings, etc. are fitted for lifting by chains with hooks.

## 27. MACHINERY AND KINDRED TRAFFICS—*continued.*

- (ii) (c) Protective packing and, where necessary, stretcher bars must be used to prevent fragile articles being crushed.
- (d) The printed, written or stencilled instructions on the machinery or covering document must be carried out.
- (iii) *Loading and Unloading.*
  - (a) A suitable vehicle must be used.
  - (b) The brakes of the vehicle must be securely applied. When loading or unloading heavy traffic and at other times, as necessary, scotches must be placed against the wheels of the vehicle.
  - (c) Packing timbers and scotches used must be in good condition and all nails should be removed from places where scotches have to be fixed so that they may bed down properly. Receiving points should endeavour to remove all nails when taking off packing.
  - (d) The weight of the load must, as far as practicable, be evenly distributed.
  - (e) The load must be secure.
  - (f) Projecting or delicate parts must be protected.
  - (g) Care must be exercised in using the ropes, chains and packing; for top-heavy machines additional ropes or chains must be used to secure from ends and sides.

### (iv) *Loading Gauge.*

Vehicles loaded with threshing machines, agricultural and traction engines, or with any other traffic when there is doubt about dimensions, must be passed under the load testing gauge.

Should there be any doubt, having in mind the route and destination, of any load being within gauge throughout, the District Commercial Officer should be consulted.

### (v) *Weighing of heavy traffic.*

Machines, traction engines and other traffic which cannot be weighed at forwarding point must be sent to the nearest available point *en route* which has a wagon weighbridge of sufficient capacity, with instructions to "weigh and advise". If the wagon is found, at the weighing point, to be overloaded or otherwise unfit for travel it must not be sent forward.

### (vi) *Labelling of Wagons.*

"Shunt with care" labels must be used.

## 28. MALT.

Malt must, whenever possible, be loaded in non-ventilated vans. Such stock must be ordered for traffic to be loaded in private sidings.

## 29. PAPER, WASTE, PRESSED IN BALES.

High goods and suitable mineral wagons must be used.

The highest full tier must have at least two-thirds of its depth below the wagon rave.

Further bales may be loaded lengthwise through the centre of the load to form a tilt.

All loads must be securely sheeted and roped.

### 30. PAPER.

On acceptance each reel, bale or bundle must be examined and if there are any defects, particularly tears in the outer covering, or denting at the ends of reels, a qualified signature must be given. Particulars of the defects must be noted on the consignment note.

Traffic likely to cause damage must not be placed near consignments of paper.

#### (A) Paper in Reels.

Reels must be dealt with "on the roll" ("rollin wise"); they must not be placed on end.

For turning reels, other than "rollingwise", on wagon, warehouse or road vehicle floors, a wooden slipper must be used.

For levering reels into position, a wooden lever or "bat" may be used as necessary.

##### (i) *Cranage.*

- (a) Endless rope sling to be used, the splice being kept against the crane hook. The sling to be laid on the floor of vehicle or deck in position so that the reel may be rolled centrally on to it as shown in diagram 1.

*Diagram 1.*



- (b) If the rope sling cannot be applied in this way, the reel should be lifted sufficiently at one end—with "L" or inverted "Y" or other suitable type lifting shoe—to enable the sling to be placed in position.
- (c) After lifting, if the reel cannot be rolled off the sling, it should be lifted sufficiently at one end with the shoe so that the sling may be withdrawn.
- (d) Care must be taken to prevent the edges of reels from hitting the wagon frame or other obstructions.
- (e) Cranes must not be used to drag reels or to remove slings from them.

##### (ii) *Loading rail vehicles.*

- (a) Open wagons, suitable for the kind and number of reels to be conveyed, should be used. Those with grease axle boxes, uneven floors or bottom doors are unsuitable.

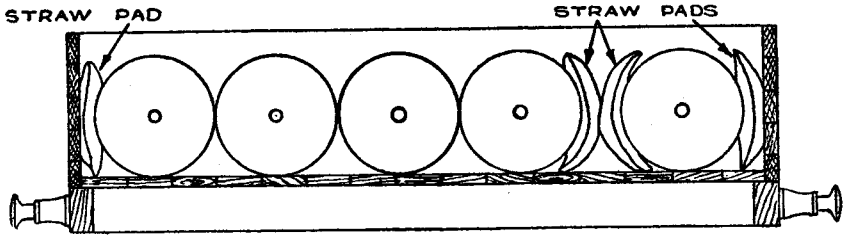
Wagons should be clean, with dry floors, free from projecting nails. Floors should be covered with straw to keep outer coverings clean.

30. PAPER—continued.

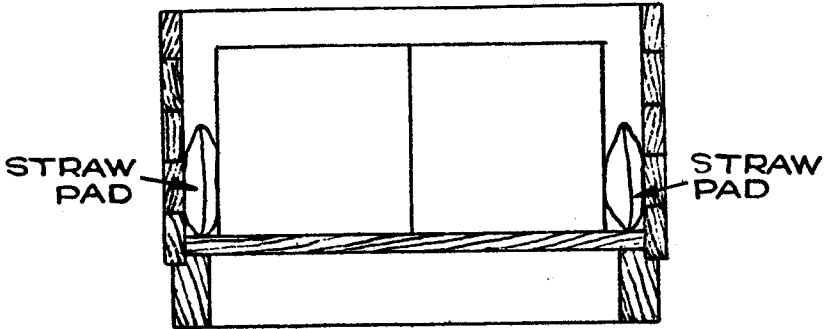
(A) (ii)

- (b) Reels are to be loaded “rollingwise” in one, two or three tiers. Those on the floor of the wagon must be nested close together. Ends and sides must be firmly packed with straw or straw pads (see diagram 2).

Diagram 2.



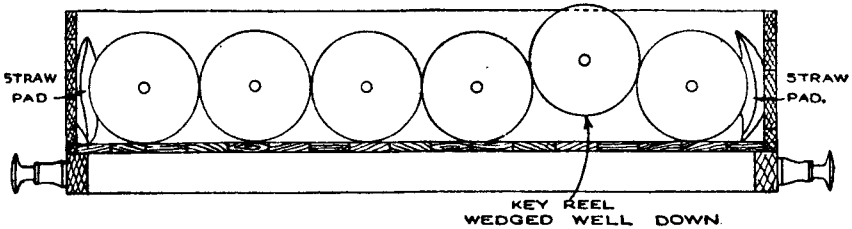
SIDE VIEW



CROSS SECTION

- (c) When reels do not make a firm load on the bottom of the wagon, one key reel may be wedged well down to secure (see diagram 3).

Diagram 3.



- (d) When reels are loaded more than one tier high, the second and subsequent tiers must be nested in the saddles of the lower tiers as shown in diagram 4. When reels are tiered the rave of the wagon must always be well above the bottom of the reels in the top tier.

### 30. PAPER—continued.

#### (A) (ii)

- (e) Pairs of planed boards (with the edges rounded off to avoid cutting the reels) placed on the wagon floor so that the reels may be raised above it are an advantage for regular traffic between given points. The dimensions recommended are:—

15 ft. long; 9 ins. wide;  $1\frac{3}{4}$  ins. thick.

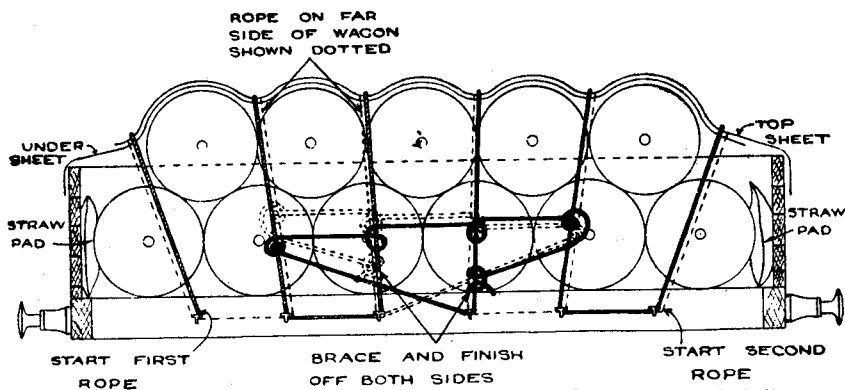
The boards should be marked with the name of the forwarding station and kept in circuit between the stations.

#### (iii) Sheeting and roping.

*Sheeting.*—All loads must be double sheeted. On double tier loads sheets must be fastened to overlap at opposite ends of the wagon (see diagram 4).

*Roping.*—Double tier loads must be secured with two ropes *over the sheets*. Each rope to be started at alternate ends of the opposite sides of the wagon, brought over the load, braced and tied off (see diagram 4).

Diagram 4.



#### (iv) Labelling of wagons.

In addition to the ordinary labels, wagons must bear "Shunt with care" labels.

#### (v) Loading cartage vehicles.

- (a) The floor of the vehicle must be clean and dry, dry straw being spread over it in wet weather.
- (b) Reels must be loaded "rollingwise" (except single reels of exceptional length which must be loaded lengthwise) and protection afforded from ends and sides of vehicles as necessary. Reels must not rest on the rave of the vehicle.

### 30. PAPER—*continued.*

#### (A) (v)

- (c) When reels are loaded more than one tier high, the second and subsequent tiers should be nested in the saddles of the lower tier, as when loading rail wagons.
- (d) Loads must be sheeted and roped as necessary.

#### (B) Paper in Bales, Bundles, etc.

Packages must be kept flat. For barrowing, a four-wheeled trolley or, if not available, a large sack barrow with a flat board or, failing this, a straw pad, placed over the blade, should be used.

Care should be taken to prevent the buckling of long bundles or parcels. If not protected by battens they should be lifted by two men.

##### (i) *Loading rail vehicles.*

- (a) Covered wagons which are clean, weatherproof, with dry floors free of projecting nails, must be used. Floors should be covered with straw to keep outer coverings clean.
- (b) To prevent movement in transit, spaces between packages or between packages and the ends or sides of wagons must be filled with straw, straw pads, or suitable soft traffic.

##### (ii) *Labelling of wagons.*

In addition to the ordinary labels, wagons must bear "Shunt with care" labels.

##### (iii) *Loading cartage vehicles.*

- (a) The floor of the vehicle must be clean and dry, dry straw being spread over it in wet weather.
- (b) Packages must be loaded flat and clear of the rave of the vehicle.
- (c) Loads must be sheeted and roped as necessary.

### 31. PITWOOD AND PIT PROPS.

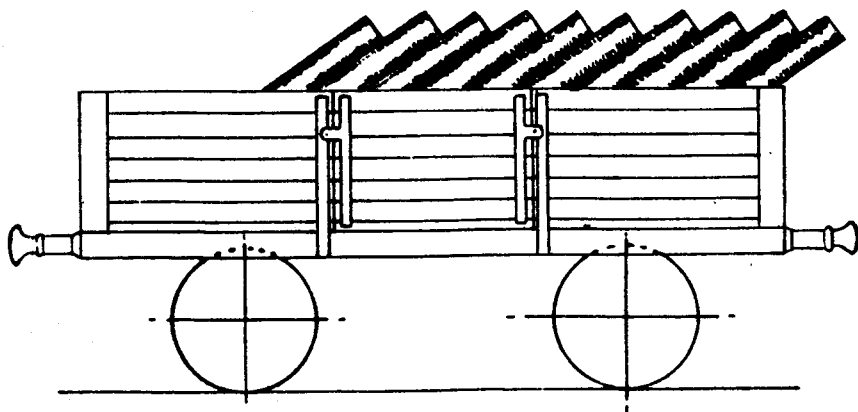
The traffic differs in shape, length and girth, and the following diagrams are supplied as guides to good and safe loading.

Standard High goods or Mineral wagons must be used.

Pitwood loaded crosswise must always be kept at least 4 inches below the rave of the wagon.

31. PITWOOD AND PITPROPS—*continued.*

*Diagram 1.*



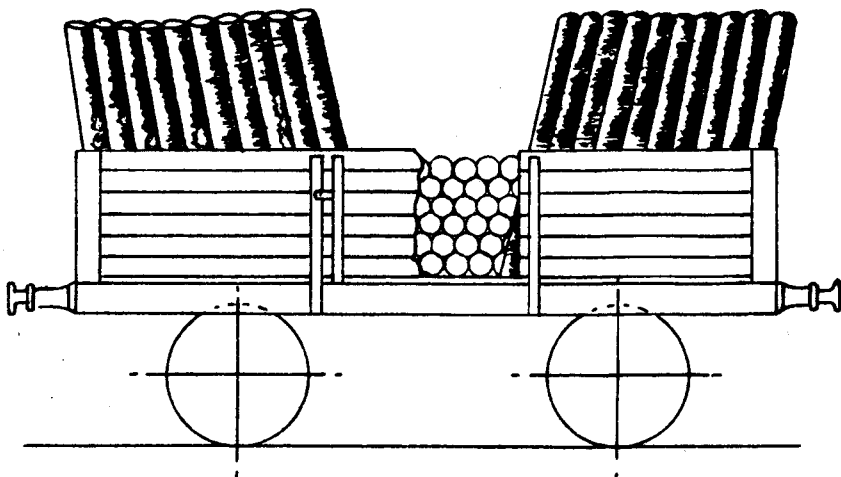
When loaded as in diagram 1 and a wagon with an end door is used, the lower end of the load must be towards the fixed end of the wagon.

The bases of all props must rest upon the wagon floor.

Loads to be roped as necessary.

*Diagram 2.*

*Alternative method for lengths not exceeding 7 feet.*



Sufficient pieces must be placed crosswise in the centre of the wagon from the bottom to well tighten the load.



### 32. PRESERVES, JAMS, CONFECTIONERY, ETC. IN JARS OR BOTTLES.

- (a) Cases and cartons must be loaded flat, right side up, lengthwise in rail or road vehicles.
- (b) Heavy articles must not be placed on top of cartons; tiers must not be so loaded as to crush lower layers.
- (c) When jars or bottles are in cases without lids, the upper cases must be so loaded that damage to the contents of cases beneath is avoided.

### 33. SALT.

Owing to its corrosive action, salt must not be loaded in steel wagons.

### 34. SCRAP METAL.

**Note.**—The use of railed coke wagons for conveying scrap metal is prohibited, as discharge by magnet crane results in damage to the rails.

#### (i) (a) *Scrap metal loaded loose.*

Must be loaded in high-sided wagons of five planks and upwards and the width and length of the load must not exceed the width and length of the wagon body. When it is necessary to load above the rave of the wagon the pieces round the sides and ends must have at least two-thirds of their bulk below the rave and they must be placed in such a way as to prevent any loose scrap from moving outside the wagon and exceeding the width or length of the body.

#### (b) *Scrap Metal in Bundles or Bales.*

When it is necessary to load bundles of scrap metal above the rave of the wagon, suitable bundles must be selected and placed on end round the sides and ends of the wagon with two-thirds of their bulk below the rave, in order to form stanchions over which bundles loaded in the interior cannot slide, the only exception being solid articles of such a weight as could be adequately supported by their own shape or surrounding material, so as to make movement in transit unlikely.

#### (c) *Tins, Loose Destructor Scrap, Turnings, Borings, Tinplate Cuttings, or other similar small scrap.*

Must be loaded in high-sided wagons of five planks and upwards and placed below the rave of the wagon.

- (ii) Where the method of loading precludes conforming to the above conditions the scrap must be loaded below the rave of the wagon with sufficient margin to avoid displacement during shunting operations.
- (iii) Before wagons are accepted and labelled for conveyance, the local railway staff must see that the loads are made in accordance with the above, secure and fit for conveyance.

### 35. TAR, CREOSOTE AND SIMILAR COMPOUNDS.

- (a) Barrels, full or empty, must be inspected to ensure that they are soundly constructed, securely bunged and do not leak. Unsound barrels must not be accepted.
- (b) The traffic must be loaded with bung holes uppermost and, whenever possible, in mineral or sleeper wagons. Covered wagons must not be used for full or empty barrels.
- (c) Before loading, the wagon floor must be liberally protected with suitable absorbent, e.g., ashes, sawdust, etc.; barrels or other containers then to be so loaded that they will not move or fall during transit.

(See also Instruction 26(B), re liquids in casks and drums.)

### 36. TARRED ROADSTONE.

This traffic must be loaded in mineral or sleeper wagons. Ordinary goods wagons are not to be used.

When the L.M.R. special roadstone wagons are used, coated (tarred) roadstone or similar "dirty" traffics must NOT be loaded in the YELLOW painted metal skips, as these have been specially cleaned for use with other traffics, e.g., dry stone, etc.

The skips for coated stone are painted brown.

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**Printed by Albert Gait Ltd., Castle Press, Grimsby.**